
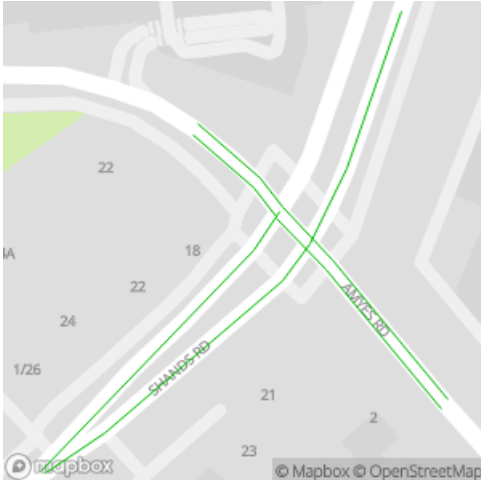


Traffic Management Plan (TMP)

Organisations	Contractor Traffic Control Systems Ltd	Principal Christchurch City Council (CCC)	RCA Christchurch City Council	
TMP Details	TMP ID CCC-T122434	Revision 1.2	TMP Status Accepted	
Worksite	Name Intersection #0503 - AMYES - GOULDING - SHANDS	Reference AMYES - GOULDING - SHANDS Site Checks	Address 21 Shands Road, Hornby, Christchurch, Canterbury, 8042, NZL	
	Worksite ID CCC-W147457	Worksite Revision 2.1	Worksite Status	

Layouts

Layout - CCC-L841859

Description	Historic site, unattended only. Once approval from Kiwi Rail given, a new TMP will be created.	
Date Range	16 Jan 2023 to 31 May 2023	
Continuous Deployment	Yes	
Traffic Control In	06:00	
Site Cleared	05:45	
First Sign In		
Pickup		
Days	Sun, Mon, Tue, Wed, Thu, Fri, Sat	
Impact Category	Longterm	
Does this layout need to be advertised?	No	
Layout Designer	Steven Haughin 8536	
Lane Closures		
Signage Required		
Traffic Impacts	Bus Route / Stop Changes	
UTMD		

Cannot Affect Road Assets

1. It is the responsibility of the client to arrange or have in place any permits required for works that affect road assets. Permits include Corridor Access Request (CAR), Works Access Permit (WAP), Temporary Use of Legal Road, License to Occupy and Construction Zones. For Christchurch City Council assets, Contact the CCC Asset Protection Team – Asset.Protection@ccc.govt.nz For NZ Transport Agency (NZTA) assets contact Southern Link Canterbury – either Phillipa Hunt phillipa.hunt@wsp.com 0212447573 or Robyn Murphy robyn.murphy@wsp.com 027 211 5983.
2. If the works are within the rail corridor (i.e. when on KiwiRail land or within 100m of the rail track approach at a level crossing) contact must be made with the National Permit Office at nationalpermits@kiwirail.co.nz or go to <https://www.kiwirail.co.nz/how-can-we-help/access-the-rail-corridor/permit-to-enter/applying-for-a-permit-to-enter/> for more information. This TMP is not valid when above condition is not met.
3. Where tram lines or overhead conductors are impacted as a result of works or activities on or adjacent to tram tracks (within 2.5m), Contact must be made with the Christchurch tram operator and any required forms submitted for approval / must be accepted by the Christchurch Tramways prior to activities commencing. Should no contact be made with Christchurch Tramways or approval not be granted to impact tram tracks, TMP approval is voided. CHRISTCHURCH TRAMWAY LIMITED Phone: 03 366 7830 Fax: 03 366 6943

Traffic Management Plan (TMP) General Conditions

4. This is an accepted Traffic Management Plan (TMP) from a CCC, Traffic Management Coordinator. Acceptance is conditional on the requirements below, and to any notes added to the TMP. Failure to comply with any of these requirements may cause the TMP acceptance to be revoked.
5. Collaboration Expected - The client and contractor must collaborate with any other clients/contractors that submit future TMP requests for the same location. CCC expects all parties to work together to advance their projects and notes that compromises and changes to the TMP may be required at times.
6. Temporary Traffic Management (TTM) Principles - The TMP must be deployed in accordance with the principles contained in CoPTTM and/or CCC Local Operating Procedures (LOP), and the CCC Construction Standard Specification (CSS).
7. Network Impacts - Activities on any road must be planned so as to cause as little disruption, delay or inconvenience to road users as practicable without compromising safety. The length, width and duration of any TTM must be restricted to the minimum required for the safe operation of the activity.
8. Real Time Operations Contact - The Real Time Operations team must be contacted prior to and during any work within 50 metres of a set of traffic lights. Refer to CCC Local Operating Procedures for further details. Real Time Operations contact number (03)941-8620
9. Recycling and Waste Collections - Where the TTM prevents normal collections from occurring, the contractor must either enable collection vehicle access through their worksite or pre-arrange alternative collection points.
10. Public Notifications - Notifications must be carried out a minimum of 5 working days prior to the installation of the accepted TMP. A record must be kept of who, when, and how the notifications were made. Notifications must be tailored to communicate the impacts of the work to affected stakeholders. Where impacted, the stakeholders receiving notifications must include: bus operators, businesses, private residences, schools, hospitals, and emergency service providers. All letter drops must be emailed to : intel.canterbury.district@police.govt.nz CanterburyDistrict-DLT@fireandemergency.nz snradmincanterburyfieldoperations@stjohn.org
11. Parking Meters - Where metered car parks cannot be used because of work being undertaken, the Client must pay the associated costs for loss of revenue by contacting the CCC Parking Operations Team. email : Parking@ccc.govt.nz Payments must be arranged before the accepted TMP can be installed. Note if a Time Limit car park or loading zone is required to be used as part of the worksite, you may be required to provide a Time Limit car park or Loading Zone in an alternative location for public use. Please note: where parking restrictions are deployed for the sole purpose of parking worker vehicles, not directly involved in work activities in the carriageway, the TMP may be rescinded as deployment of parking restrictions for worker vehicles is not an appropriate use of parking spaces.
12. During the COVID-19 Response this TMP acceptance does not constitute approval to undertake work. Any works undertaken must be in accordance with NZ Government COVID-19 Restrictions.
13. Where complaints are received by CCC as a result of works that are being undertaken, the party that the works are being undertaken for must be the primary responder to complaints received. Parties undertaking the works must deal with enquiries / issues raised by the public as a result of works and inform CCC of actions taken to enable closing out of issues.

14. Where works are undertaken that create noise that impacts stakeholders, CCC acceptance of a TMP does not grant permission to exceed noise levels as set within the Christchurch City District Plan, nor does it grant permission to create excessive noise. Should noise be generated, that exceeds levels as set in the Christchurch District Plan, or works create excessive noise, pursuant to section 326 of the Resource Management Act, the work may need to be abated immediately.
15. Where mid block Road Closures or No Entry sites are experiencing vehicles not observing advanced warning of the closure, positive traffic management measures must be increased to remove the risk of unwanted circulating traffic in close proximity to the closure point. This may require a TMP revision to be submitted.
16. Where Portable Traffic Signals are accepted within a TMP, they must be deployed in accordance with CoPTTM B5 - Vehicle actuated Traffic Signals are required to be utilised unless explicitly approved for an alternate mode use within the TMP.

Programme and Nature of Worksite Changes Conditions

17. When work is completed early, the contractor must update MyWorksites (close the TMP).
18. Where a date extension is required for an existing Worksite or TMP, the contractor must submit a revision to MyWorksites prior to the end date of the original Worksite or TMP.
19. The TMP details contained in MyWorksites must reflect the current site conditions. If the nature of the worksite or TTM impact changes significantly after TMP acceptance, then a revision to the TMP must be submitted via MyWorksites.
20. Further information can be found at: www.myworksites.co.nz or <https://ccc.govt.nz/transport/legal-road/traffic-management-news-and-information>. CCC Traffic management team can be reached by calling the CCC TM Duty Phone 7am-5pm Weekdays (03) 9418842. Outside these hours call the CCC Call centre on (03) 941 8999

Application for traffic management plan extension

A currently accepted traffic management plan is required when applying for an extension.

TMP number:	CCC-T122434
Sought by: (Contractor/Applicant)	
TTMP warranted planner drafting extension:	Steven Haughin
Client:	Christchurch  City Council
Reason for extension:	Waiting on Kiwi Rail / RTO to confirm approval.
Location details: (Road name and details)	Amyes Rd Shands Rd Goulding Ave intersection
Dates of extension request:	31 st March 2023 / 31 st May 2023
Acceptance conditions:	
Accepted by:	<div style="border: 1px solid green; border-radius: 10px; padding: 5px; background-color: #e0f0e0;"><p style="margin: 0;">APPROVED</p><p style="margin: 0; font-size: small;">By Daniel Nolan ID#28787 STMS (ABC)-NP on behalf of CCC at 10:34 am, Mar 24, 2023</p></div>
If a road closure extension is proposed to be extended, an updated road closure application form must be filled in and attached.	
<p>Approval is granted on the basis that the TMP is relevant to the work being carried out onsite, at the time of the extension application.</p> <p>Should an extension of Road Space be required, a full revision including updated plans will be required.</p> <p>This approval now forms part of your accepted TMP and is required to be held onsite for inspection.</p> <p>This TMP extension is deemed to be approval to extend all Temporary Speed Restriction in place within this TMP.</p>	








TMP No: STM096 Amyes Rd Shands Rd Goulding Ave

Steven Haughin (Waka Kotahi TTM Planner)
Monday, 16 January 2023

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TRAFFIC MANAGEMENT PLAN (TMP) – FULL FORM

Job # RAMM 3172 Rev 0

Organizations /TMP reference	TMP reference: STM096	Contractor (Working space): 	Principal (Client): Christchurch City Council 		
	TMP CCC-T122434	Contractor (TTM): 	RCA: Christchurch City Council 		
Location details and road characteristics	Road names and suburb		House no./RPs (from and to)	Road level	Permanent speed
	Amyes Rd, Hornby Shands Rd, Hornby Goulding Ave, Hornby		Tower St to Shands Rd Lesley Keats Pl to Main Sth Rd Main Sth Rd to Shands Rd	L2 L2 L1 Cat A	
Traffic details (main route)	AADT from Council data Amyes Rd 18,200vpd Shands Rd 20,000vpd Goulding Ave 5,600vpd		Peak flows <ul style="list-style-type: none">Mon – Thurs: 7am – 9am, 4pm – 6pmFriday: 7am – 9am, 3.30pm – 6pm		

Description of work activity

UNATTENDED WORKSITE

This is a historic worksite that is requiring replacement of traffic signal pole #10 to the end of the median island on Amyes Rd at Shands Rd Goulding Ave intersection.

This TMP is for the unattended worksite only. A new TMP will be submitted once approval to work within this area has been given by Kiwi Rail (including repair work to the central median island).

Planned work programme

Start date	16 January 2023	Time	7am	End date	31 March 2023	Time	5.30pm
Consider significant stages, for example: road closures detours no activity periods.	This is a historic worksite will TTM already installed. No active work will be undertaken with this TMP. Unattended periods – day and night as per TMD-3172-R01 (cones around median island).						
Alternative dates if activity delayed	Not required. This TMP is for the unattended worksite only. A new TMP will be submitted once approval to work within this area has been given by Kiwi Rail.						

Road aspects affected (delete either Yes or No to show which aspects are affected)

Pedestrians affected?	No	Property access affected?	No	Traffic lanes affected?	No
Cyclists affected?	No	Restricted parking affected?	No	Delays or queuing likely?	No

Proposed traffic management methods

Installation <i>(includes parking of plant and materials storage)</i>	<p>Site Location</p> <p>This is an existing worksite from 2022. The site is established for <i>unattended works</i>. If another contractor will impact the worksite, the STMS' <i>MUST</i> communicate and agree integration of worksites and who is responsible for which area. Each TMP must then be amended accordingly and signed by the STMS'.</p>
Attended (day)	<p>Not Required</p> <p>This TMP is for an unattended worksite only. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval. All trafficable lanes are open and clear. All footpaths are open and clear. All crossing points are open and clear.</p>
Attended (night)	<p>Not Required</p> <p>This TMP is for an unattended worksite only. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval. All trafficable lanes are open and clear. All footpaths are open and clear. All crossing points are open and clear.</p>
Unattended (day)	<p>This TMP is for an unattended worksite only. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval. All trafficable lanes are open and clear. All footpaths are open and clear. All crossing points are open and clear.</p>
Unattended (night)	<p>This TMP is for an unattended worksite only. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval. All trafficable lanes are open and clear. All footpaths are open and clear. All crossing points are open and clear.</p>
Detour route	<p>Not Required</p> <p>Does detour route go into another RCA's roading network? No If Yes, has confirmation of acceptance been requested from that RCA? No Note: Confirmation of acceptance from affected RCA must be submitted prior to occupying the site.</p>
Removal	<p>Not Required</p> <p>The worksite will not be removed under this TMP. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval which will include the removal process.</p>

Proposed TSLs (see TSL decision matrix for guidance)

	TSL details as required Approval of Temporary Speed Limits (TSL) are in terms of Section 7 of Land Transport Rule: Setting of Speed Limits 2022 (List speed, length and location)	Times (From and to)	Dates (Start and finish)	Diagram ref. no's (Layout drawings or traffic management diagrams)
Attended day/night	A temporary maximum speed limit of km/h is hereby fixed for motor vehicles travelling over the length of m situated between (House no./RP) and (House no./RP) on (street or road name)	Not Required	Not Required	Not Required
Unattended day/night	A temporary maximum speed limit of km/h is hereby fixed for motor vehicles travelling over the length of m situated between (House no./RP) and (House no./RP) on (street or road name)	Not Required	Not Required	Not Required
TSL duration	Will the TSL be required for longer than 12 months? <i>If yes, attach the completed checklist from section I-18: Guidance on TMP Monitoring Processes for TSLs to this TMP.</i>			No

Positive traffic management measures

As all lanes and footpaths are clear and open, no positive traffic management measures will be required.

Contingency plans

Generic contingencies for: major incidents incidents pre-planned detours. <i>Remove any options which do not apply to your job</i>	Major Incident A major incident is described as: → Fatality or notifiable injury - real or potential → Significant property damage, or → Emergency services (police, fire, etc.) require access or control of the site.	Actions The STMS must immediately conduct the following: → stop all activity and traffic movement → secure the site to prevent (further) injury or damage → contact the appropriate emergency authorities → render first aid if competent and able to do so → notify the RCA representative and / or the engineer → under the guidance of the officer in charge of the site, reduce effects of TTM on the road or remove the activity if safe to do so → re-establish TTM and traffic movements when advised by emergency authorities that it is safe to do so → Comply with any obligation to notify WorkSafe.

	<p>Incident</p> <p>An incident is described as:</p> <ul style="list-style-type: none"> → excessive delays - real or potential → minor or non-inquiry accident that has the potential to affect traffic flow → structural failure of the road. 	<p>Actions</p> <p>The STMS must immediately conduct the following:</p> <ul style="list-style-type: none"> → stop all activity and traffic movement if required → secure the site to prevent the prospect of injury or further damage → notify the RCA representative and / or the engineer → STMS to implement a plan to safely remove TTM and to establish normal traffic flow if safe to do so → re-establish TTM and traffic movements when it is safe to do so and when traffic volumes have reduced.
	<p>Detour</p> <p>If because of the on-site activity it will not be possible to remove or reduce the effects of TTM once it is established a detour route must be designed. This is likely for:</p> <ul style="list-style-type: none"> → excessive delays when using an alternating flow design for TTM → redirecting one direction of flow and / or → total road closure and redirection of traffic until such time that traffic volumes have reduced, and tailbacks have been cleared. <p>The risks in the type of work being undertaken, the risks inherent in the detour, the probable duration of closure and availability and suitability of detour routes need to be considered.</p> <p>The detour and route must be designed including:</p> <ul style="list-style-type: none"> → pre-approval from the RCA's whose roads will be used or affected by the detour route → ensure that TTM equipment for the detour-signs etc. are on site and pre-installed. 	<p>Actions</p> <p>When it is necessary to implement the pre-planned detour the STMS must immediately undertake the following:</p> <ul style="list-style-type: none"> → Notify the RCA and / or the engineer when the detour is to be established → Drive through the detour in both directions to check that it is stable and safe → Remove the detour as soon as it practicable and safe to do so and the traffic volumes have reduced, and tailbacks have cleared → Notify the RCA and / or the engineer when the detour has been disestablished and normal traffic flows have resumed.
	<p>Note also the requirements for no interference at an accident scene:</p> <p>In the event of an accident involving serious harm the STMS must ensure that nothing, including TTM equipment, is removed or disturbed and any wreckage article or thing must not be disturbed or interfered with, except to:</p> <ul style="list-style-type: none"> → save a life of, prevent harm to or relieve the suffering of any person, or → make the site safe or to minimize the risk of a further accident; or → maintain the access of the general public to an essential service or utility, or → prevent serious damage to or serious loss of property, or → follow the direction of a constable acting in his or her duties or act with the permission of an inspector. 	
<p>Other contingencies to be identified by the applicant <i>(i.e. steel plates to quickly cover excavations)</i></p>	<p>If the job changes, stop and re-evaluate.</p> <p>Most likely: Replacement of cones with bollards (approval from Kiwi Rail is required for this to proceed).</p>	

Authorisations

Parking restriction(s) alteration authority	Will controlled street parking be affected?	No	Has approval been granted?	No
Authorization to work at permanent traffic signal sites	Will portable traffic signals be used, or permanent traffic signals be changed?	Yes	Has approval been granted?	Yes No
	<p>All work within 50m of a signaled intersection must be notified to CCC's Real Time Operations (RTO) team. This includes sign deployment and detouring significant volumes of traffic through signaled intersections.</p> <p>RTO contact details,</p> <ul style="list-style-type: none"> ☎ 03 941 8620 (6am to 6pm and for emergencies), or → xxxxxxx@xxx.xxvt.nz <p>RTO notification time frames,</p> <ul style="list-style-type: none"> → 24-48 hours before work commences (email preferred) ☎ At time of deployment (phone call preferred) – For night deployments, provide confirmation of planned deployment during the preceding business hours, ☎ 24-48 hours before a major change or disestablishment (phone call preferred) 			
Road closure authorization(s)	Will full carriageway closure continue for more than 5 minutes (or other RCA stipulated time)?	No	Has approval been granted?	No
Bus stop relocation(s) – closure(s)	Will bus stop(s) be obstructed by the activity?	No	Has approval been granted?	No
	<p>Works are on the following bus route,</p> <ul style="list-style-type: none"> → 125 with no stops impacted. 			
Authorization to use portable traffic signals	Make, model and description/number	Not required.		
	NZTA compliant?	No		
EED				
Is an EED applicable?	No	EED attached?	No	
Delay calculations/trial plan to determine potential extent of delays				
No delays dues to all trafficable lanes clear and open.				
Public notification plan				
Traffic Control Systems will be required to meet all the requirements stipulated by Christchurch City Council and Kiwi Rail in respect to public notifications.				
Public notification plan attached?	No			

On-site monitoring plan

Attended <i>(day and/or night)</i>	This TMP is for an unattended worksite only. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval.
Unattended <i>(day and/or night)</i>	The STMS is to ensure worksite inspections of all TTM are completed a minimum of once in every 24hr period unless otherwise stated by the RCA.

Method for recording daily site TTM activity (e.g. CoPTTM on-site record)

The attached "On-Site Record" sheet is to be used to record the monitoring of the TTM to ensure the traffic management measures remain fit for purpose, suitable, installed and used correctly. Monitoring will follow the prompts provided on the recording sheet, and if multiple STMS' check this site, each STMS must initial and sign for the respective times.

All audits and site record sheets must be retained for a minimum of twelve months.

Site safety measures


Not required.
This TMP is for an unattended worksite only. Once approval has been given by Kiwi Rail, a new TMP will be submitted for approval.

Temporary safety barrier system	Will a temporary safety barrier system be used at this worksite?	No	If yes, has the temporary safety barrier system been designed by an installation designer and independently reviewed as being fit for purpose?	No
	Statement from temporary safety barrier installation designer attached			Not attached

Other information

All incidents onsite are to be reported to the RCA by completing a Traffic Incident report. Contact must be made with the RCA within 24hrs with relevant documents provided below and sent to xxx.xxxxxxx@xxx.xxvt.nz

→ Approved TMP, Photos of the site, Incident report form, Crash diagram and Onsite record



Where work activities fall within the rail corridor area, the Network Control Manager is to be notified on 04 498 2066 to determine whether a Rail Protection Officer is required to be on-site.

- All traffic management plans where the traffic management or work activity is within 10m of the track, including work at level crossings, must be notified to KiwiRail www.kiwirail.govt.nz
- All work within 5m of track requires a KiwiRail Permit to Enter www.kiwirail.govt.nz

Site specific layout diagrams

Number	Title
TMD-3172-R01	Amyes Rd Shands Rd Goulding Ave - Unattended
TMD-3172-R02	Bus Route




Site specific / generic mobile closure diagrams

Number	Title

RCA consent (eg CAR/WAP) and/or
RCA contract reference

Intersection #0503 - AMYES - GOULDING - SHANDS - CCC-W147457

Contact details

	Name	24/7 contact number	CoPTTM ID	Qualification	Expiry date
Principal	Christchurch City Council 	Jann Kuhlmann 027 310 4266			
TMC	Craig Halkett - CCC Dan Nolan - CCC Fiona McCallum - CCC Keith Smith (CCC) Shaw Ritson - CCC Simon Hodges - CCC Teri Lloyd - CCC Duty TMC on call from 7am-5pm Monday to Friday Call Centre (24hrs)	027 497 0271 027 211 8748 027 250 7709 027 274 6870 027 203 9256 027 497 1025 027 200 7569 03 941 8842 03 941 8999	22437 28787 95664 68165 59768 33727 22807	A, B & C NP A, B & C NP A, B & C NP L 2/3 P A, B (C NP) A, B & C NP A, B & C NP	2/11/24 29/04/24 25/10/25 12/04/24 29/04/24 21/06/25 21/06/25
Engineers' representative	Not required				
Contractor		Charlotte Pulley 03 338 2305			
STMS		03 338 2305 (Option 2)			
TC	Additional STMS's / TC's / TMO's may use this agreement if their warrant card is current and undertaking work on behalf of Traffic Control Systems	03 338 2305			
Others as required	Bex Shirley – Ops Manager RTO – Traffic Signals Kiwi Rail	022 566 9218 03 941 8620 04 498 2066			

TMP preparation

Preparation	Steven Haughin	16/01/2023		08536	L2/3 NP & TTM Planner	23/07/2023
	Name (STMS qualified)	Date	Signature	ID no.	Qualification	Expiry date
This TMP meets CoPTTM requirements				Number of diagrams attached		2
TMP returned for correction (if required)						
	Name	Date	Signature	ID no.	Qualification	Expiry date

Engineer/TMC to complete following section when approval or acceptance required

Temporary safety barrier system	The attached temporary safety barrier design has been independently reviewed as being fit for purpose	Not Required				
TMP Approved	<div style="border: 2px solid #4CAF50; border-radius: 15px; padding: 10px; display: inline-block;"> <p style="font-size: 24px; margin: 0;">APPROVED</p> <p style="font-size: 10px; margin: 0;">By Daniel Nolan ID#28787 STMS (ABC)-NP on behalf of CCC at 9:32 am, Jan 19, 2023</p> </div>					
	Name	Date	Signature	ID no.	Qualification	Expiry date
Acceptance by TMC (only required if TMP approved by engineer)	Name	Date	Signature	ID no.	Qualification	Expiry date

Qualifier for engineer or TMC approval

Approval of this TMP authorizes the use of any regulatory signs included in the TMP or attached traffic management diagrams.

This TMP is approved on the following basis:

1. To the best of the approving engineer's/TMC's judgment this TMP conforms to the requirements of CoPTTM.
2. This plan is approved on the basis that the activity, the location and the road environment have been correctly represented by the applicant. Any inaccuracy in the portrayal of this information is the responsibility of the applicant.
3. The TMP provides so far as is reasonably practicable, a safe and fit for purpose TTM system.
4. The STMS for the activity is reminded that it is the STMS's duty to postpone, cancel or modify operations due to the adverse traffic, weather or other conditions that affect the safety of this site.

Notification to TMC prior to occupying worksite/Notification completed

Type of notification to TMC required	Works Notification spreadsheet/ TTM Activity Master weekly-daily spreadsheet	Notification completed	Date <input style="width: 100%;" type="text"/>
			Time <input style="width: 100%;" type="text"/>

RCA consent (eg CAR/WAP) and/or RCA contract reference	Intersection #0503 - AMYES - GOULDING - SHANDS - CCC-W147457
--	--

ON-SITE RECORD (On-site record must be retained with TMP for 12 months)	Today's date	
--	--------------	--

Job details	Job number:	Client:	TMP reference number:
-------------	-------------	---------	-----------------------

Extent of worksite	Road names & suburb		Road names & suburb	
	From:	To:	From:	To:
	From:	To:	From:	To:

Working space

Person responsible for working space		
	Name	Signature

Where the STMS/TC is responsible for both the working space and TTM they sign above and in the appropriate TTM box below

TTM

STMS in charge of TTM					
	Name	TTM ID Number	Warrant expiry date	Signature	Time
Worksite handover accepted by replacement STMS					
	Name	ID Number	Warrant expiry date	Signature	Time
	Tick to confirm handover briefing completed				

Delegation

Worksite control accepted by TC/STMS-NP					
	Name	ID Number	Warrant expiry date	Signature	Time
	Tick to confirm briefing completed				

Temporary speed limit

Street/road name (RPs or street numbers):	TSL action	Date:	Time:	TSL speed:	Length of TSL (m):
From: To:	TSL installed				
	TSL remains in place				
	TSL removed				

Street/road name (RPs or street numbers):	TSL action	Date:	Time:	TSL speed:	Length of TSL (m):
From: To:	TSL installed				
	TSL remains in place				
	TSL removed				

Street/road name (RPs or street numbers):	TSL action	Date:	Time:	TSL speed:	Length of TSL (m):
From: To:	TSL installed				
	TSL remains in place				
	TSL removed				

Worksite Monitoring

TTM to be monitored and 2 hourly inspections documented below.

Items to be inspected	Post setup /arrival	2 hourly check	2 hourly check	2 hourly check	2 hourly check	2 hourly check	Removal /off-site	24 hours
High-visibility garment worn by all?								
Signs positioned as per TMP?								
Conflicting signs covered?								
Correct delineation as per TMP?								
Lane widths appropriate?								
Appropriate positive TTM used?								
Footpath standards met?								
Cycle lane standards met?								
Traffic flows OK?								
Adequate property access?								
Barrier deflection area is clear? <i>(Refer to Barrier design statement)</i>								
Others								
Time inspection completed (24hr):								
Signature:								

Comments

Time (24hr)	Adjustment made and reason for change

Quick reference checklist for FULL crew briefing

1 STMS role

- Name, role and authority

2 Personal Safety

- Assembly / evacuation point
- First aid
- Closest medical centre
- PPE gear
 - Hi Viz (compliant, worn correctly, acceptable condition)
 - Other PPE (as required) by RCA,
 - Waka Kotahi and company

3 Crew duties

- Vehicles to be used
- Roles of TTM crew
 - AWWMS driver
 - Shadow vehicle driver
 - Work vehicle driver
 - Deck crew
 - Note: deck crew ride in cab of work
 - Vehicle for loops

4 TMP for worksite

- Location of TMP (who has it)
- Explain any EED's
- Hand out relevant parts (e.g., TMD to be installed)

5 Activity and closure

- What the activity is (install, modify or remove TTM)
- Type of closure (e.g., lane drop/merge, alternating flow)

6 Risk assessment for the task

- Hazards/risks to be aware of (e.g., traffic speed, sun glare, wet conditions, slippery deck, other identified site hazards)
- Also explain the controls that are in place to manage the hazards

7 Safety (no go) zones

- No go areas/safety zones e.g.,
 - 10m roll ahead
 - 1m lateral safety zone
 - No going into live lane

8 Procedure to be followed

- Go to diagram(s) and summarise layout and sequence of mobile operation
- Include:
 - Tail pilot/AWWMS location, display
 - Shadow vehicle location, position of vehicle to enable crew to safely exit and enter the work vehicle, pad down, display
 - Cover the TTM sequence. Use the procedures in the TMP as the basis for this part of your briefing

9 Contingency plans

- Briefing to include details on contingencies and actions e.g., weather, delays, emergency services through the site, traffic incidents (crashes/breakdowns), spillage of hazardous substances, other site-specific contingencies

10 Communication and Comms check

- Ensure TTM crew have their radio sets
- Inform crew of channel
- Confirm call signs
- Complete comms check
- Explain emergency call e.g., emergency, emergency, emergency then everybody follows my instructions
- I will have my phone for calls to TOC, Police, TMC, etc
- If the radios fail, contact me by phone to confirm our return to the assembly point. My phone number is _____

11 Signing the hazard register/briefing sheet

- Check for questions from TTM crew
- Get them to sign your company's hazard register/briefing sheet

4 Worksites impacting signalised intersections

Contractors planning for works to occur in or near a signalised intersection (generally within 50 metres or greater) must contact the Council's Real Time Operations (RTO) Team during the planning phase of works to discuss proposed works. This is to enable the RTO Team to identify any changes that may be required due to intersection functionality and layout. This discussion must occur with a minimum of two weeks before the scheduled start date on site, to enable pre-planning and any signal controller personality changes to be programmed. Please note: there may be a charge for accessing services provided by the Council's RTO Team.

Under the Transport Technology Maintenance Contract between CCC and TCS, this notification of 2 weeks prior is not required.

RTO Team contact

The RTO Team's operating hours are Monday to Friday 6am to 6pm.

The duty number for contact during operational hours is 03 941 8620, or email signals@ccc.govt.nz

Pre-Notification of scheduled deployment of works at a signalised intersection

Once the contractor has received the accepted TMP, the RTO Team must be emailed to confirm the planned work dates and times. Please ensure emails are sent a minimum of two business days prior to planned work starting onsite.

Notification email must include:

- The TMP reference number.
- Specific intersection details and impacts that are covered in the TMP, including a detailed plan or drawing to help explain changes, as previously agreed with the RTO Team during consultation.

Notification to confirm scheduled installation or removal of works at a signalised intersection

The RTO Team must be notified to enable signal phasing alterations or to schedule returning of signals to the normal operating condition. See table below for required notification time frames.

Time of deployed TTM removal	RTO Team must be contacted
Weekday "day" 6am–6pm	Before 2pm on the day before removal
Weekday "night time" 6pm–6am, following day	Before 2pm on the day before removal
Weekend from 6pm Fri to 6am Mon	Before 12noon on the Friday preceding removal

Notifications to confirm physical deployment of works at a signalised intersection

The RTO Team must be notified by phone before physical deployment of TTM at an intersection deployment. See table below for required notification time frames.

Time of TTM deployment	RTO Team must be contacted
During RTO Team operational hours (Mon–Fri 6am–6pm excluding public holidays)	Immediately before TTM deployment
Weekday – nighttime	Before 2pm on the day of deployment
Weekend – day or night	Prior to 12noon on the Friday preceding deployment

4 continued

Covering of traffic signal lanterns

Where signal shrouds are required, due to TTM deployments altering signal operations, conflicting lanterns must be covered or completely obscured so they don't create the potential for road user confusion.

The material used to cover the lanterns must meet NZTA P43 Specifications for Traffic Signals. Council prefers that the material used to cover lanterns is a light/mid blue colour.

Working at signalised intersection when signals phasing has been altered – including Flashing Yellow (FY)

When a pre-arranged Flashing Yellow, or other alteration of signal phasing, has been agreed to by the RTO Team for works that are outside the Council's standard operational hours, the STMS responsible must be on site. The TMP must also have been installed as agreed, at the agreed time as the signals will change to the altered function.

The STMS must also remain onsite until the scheduled time for the signals to return to normal phasing.

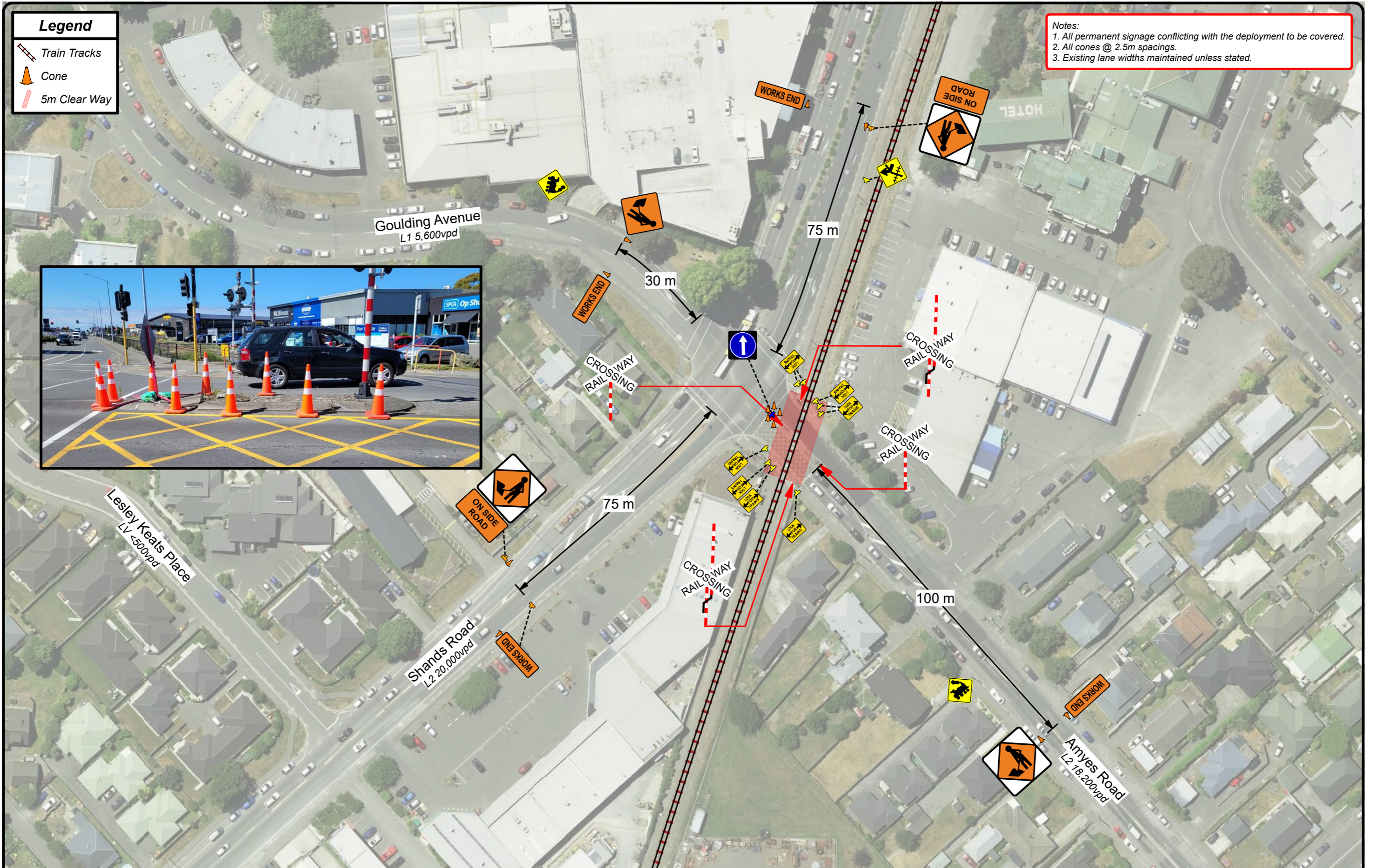
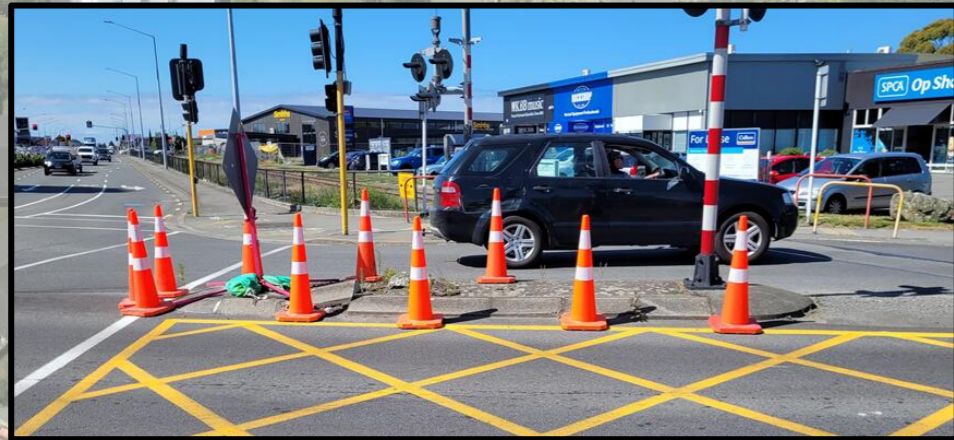
The RTO team do not operate outside of normal operational hours shown above, unless prearranged and the contractor has organised to pay for the additional out of hours service.

Legend

- Train Tracks
- Cone
- 5m Clear Way

Notes:

1. All permanent signage conflicting with the deployment to be covered.
2. All cones @ 2.5m spacings.
3. Existing lane widths maintained unless stated.



Plan Scale

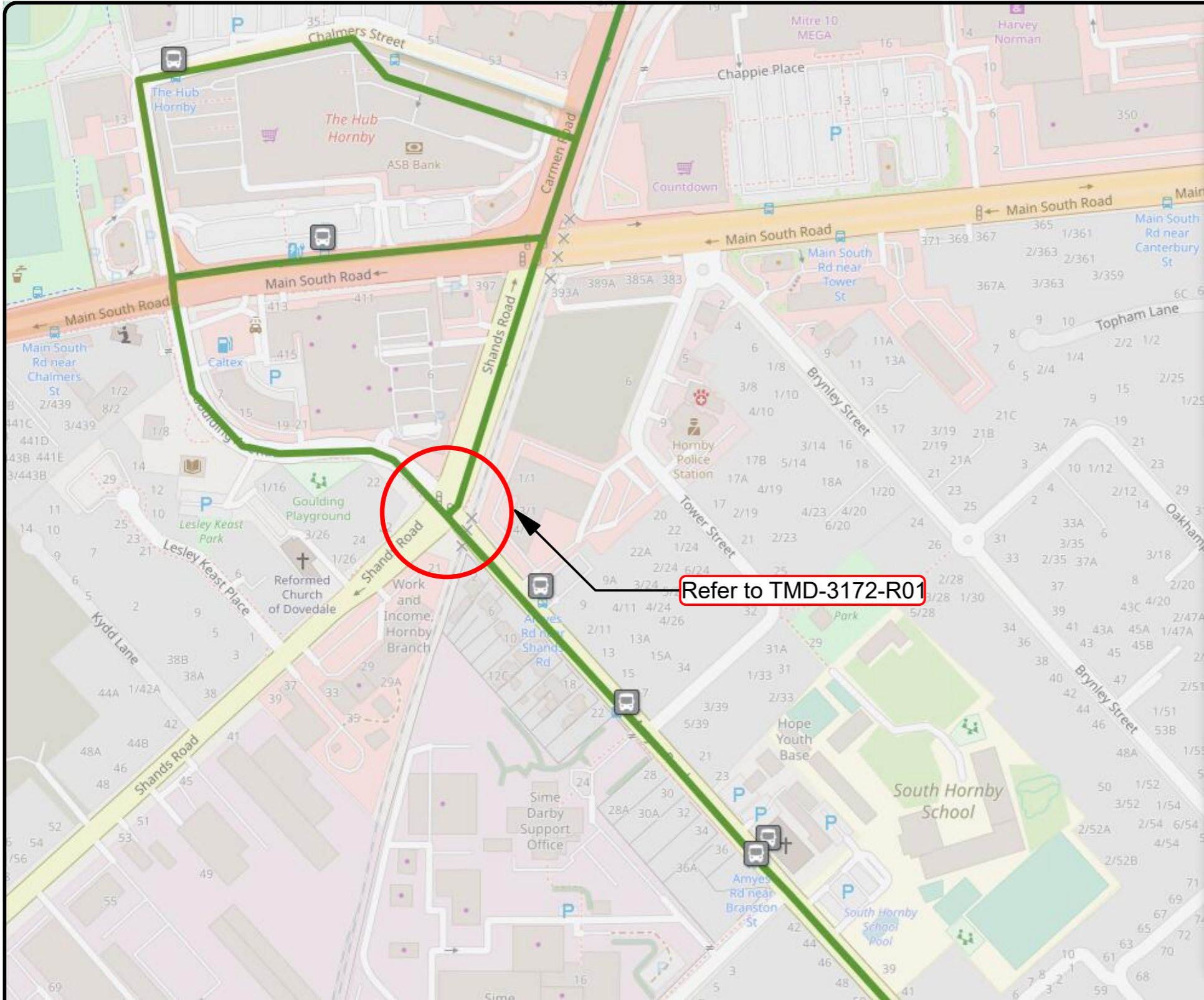
 1 : 1000 (1cm = 10m)

AUTHOR
STEVEN HAUGHIN
 16th JANUARY 2023
 WARRANT CARD ID# 8536

CONTRACTOR

DRAWING TITLE
**AMYES / SHANDS
 TRAFFIC SIGNAL WORK
 UNATTENDED**

TMP # STM096	SHEET 1 of 1
DRAWING NUMBER TMD-3172-R01	



Search for bus stop or route

Nearby Favourites All routes

- 120 Burnside/Spreydon
- 125 Redwood/Westlake
- 130 Hei Hei/Avonhead
- 135 The Palms/New Brighton
- 140 Russley/Mt Pleasant
- 155 Eastgate/Lyttelton
- 820 Burnham/Lincoln via Rolleston
- F Diamond Harbour Ferry
- Oa Orbiter
- Oc Orbiter



Plan Scale
1 : 542 (1cm = 5m)

A3

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DRAWING TITLE
**AMYES / SHANDS
TRAFFIC SIGNAL WORK
BUS ROUTE 125**

TMP # STM096	SHEET 1 of 1
DRAWING NUMBER TMD-3172-R02	