



SH16 EB ROSEBANK RD ON RAMP LONG TERM SITE - GATE C5

Organisations	Contractor:	Principal (client):	Road Controlling Authority:
	<p>CAUSEWAY ALLIANCE working for our thriving city</p>	<p>NZ TRANSPORT AGENCY WAKA KOTAHI</p>	<p>Auckland Motorways</p>
	CAUSEWAY ALLIANCE	NZTA	AUCKLAND MOTORWAY ALLIANCE

Location details and road characteristics	ROAD NAME/S	ROAD LEVEL	PERMANENT SPEED	HOUSE NO./RPS		SUBURB
				From	To	
	SH16 Eastbound Rosebank Road on Ramp	3		016-0011-R2/0.000	016-0011-R2/0.435	Rosebank
	SH16 Eastbound Northwestern Motorway			016-0007-D/1.886	016-0000-D/9.302	

Traffic details (main route)	ROAD NAME	DIRECTION	PEAK FLOW	AADT
	Rosebank On Ramp 6800	N/A	AM Peak 670 PM Peak 560	N/A
EB Motorway 50800	AM Peak 5200 PM Peak 2950			

Description of work activity	Eastbound Rosebank Road On Ramp Long-term site for Gate C5
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Work Programme	Start Date:	25 Oct 2014	Finish Date:	25 Oct 2015
	Time:	Off Peak	Time:	Off Peak

Consider:	<ul style="list-style-type: none"> Start time (first sign out) Installation complete (activity starts) Changeover to unattended layout start Changeover to unattended layout finish Changeover to attended layout start Changeover to attended layout finish Start of removal (activity ends) Removal complete (all equipment clear)
	<p>Gate C5 access hours Mon to Fri 6am to 6pm. Sat 6am to 2pm.</p> <p>Long term site for preload material between Rosebank Rd on ramp and Causeway Bridge.</p> <p>A TC gate person is located inside the site along the haul road to coordinate truck movements.</p> <p>No escorts are required into the site as the entrance is from Rosebank Rd On Ramp with the assistance of ramp signals.</p> <p>Site exit is towards the Causeway bridge. There is a shoulder area for vehicles to use to increase their speed to merge with traffic.</p> <p>Alternatively there is a haul road that carries on to GNR off ramp where Gate C12 exit can be used.</p>

Road aspects affected	ASPECT	YES	NO	ASPECT	YES	NO
	Pedestrians affected?			✓	Restricted parking affected?	
Cyclists affected?			✓	Traffic lanes closed?		✓
Property access affected?			✓	Delays likely?		✓

Proposed Traffic Management Methods	SITE INSTALLATION	As per CoPTTM Section D, Mobile Operations				
	ACTIVE (DAY/NIGHT)	Level 3 long term shoulder closure with 80km/hr TSL and site access point				
	DETOUR ROUTE	N/A				
		Does route go into another RCA's network? (If yes, has confirmation of acceptance been requested from that RCA?)			YES	NO ✓
	INACTIVE (DAY/NIGHT)	N/A				
SITE REMOVAL	As per CoPTTM Section D, Mobile Operations					

Proposed TSL	ATTENDED DAY/NIGHT	TEMPORARY SPEED LIMIT (FROM AND TO)	TIMES (FROM & TO)	PROPOSED DATES (START & FINISH)		DRAWING NUMBER(S)
		<p>A temporary maximum speed limit of 80km/hr is hereby fixed for motor vehicles travelling over the length of 100m situated between 016-0011-R2/0.336 and 016-0011-R2/0.436 on SH16 EB Rosebank Rd On Ramp</p> <p>A temporary maximum speed limit of 80km/hr is hereby fixed for motor vehicles travelling over the length of 1535m situated between 016-0007-D/1.520 and 016-0000-D/9.302 on SH16 EB Northwestern Motorway</p>		Off Peak	25 Oct 2014	25 Oct 2015
	UNATTENDED DAY/NIGHT	No Unattended activity required				



RCA CONSENT OR CONTRACT REFERENCE	CA3695
TMP REFERENCE	100-07

Proposed Traffic Management Methods	SITE INSTALLATION <i>(includes parking of plant and materials storage)</i>	<p>The long term site TTM shall be installed under a discreet overnight closure of the onramp plus lanes 1 & 2 closed on SH16. Guardrail shall be removed to the ramp signal. Approximately 950m of TL3 temporary concrete barriers are installed between Rosebank on-ramp and the over head gantry. Access to site shall be directly after the ramp signals The lane between the concrete and steel barriers provides a safe exit point which does not require TMA assistance. The leading end of the concrete barrier is protected by a 350 Absorb crash cushion. Chevrons shall be spaced at 10m apart on top of the barriers unless they form part of a site access which requires 2.5m spacing. Site access signage shall be placed in advance and at the site entrance which may be moved along the work site to accommodate work safety zones. Lanes are narrowed to 3.1m on the mainline beside the onramp to Traherne Island where they change back to 3.25m wide. An 80kph temporary speed limit is in place. All signage will be installed on permanent posts or on light poles if available.</p> <p>In this revision signs on the overhead gantry are removed and ground mounted on a concrete block. The gantry structure is also removed by crane to allow widening works to happen. The structure is laid down in the work site and dismantled. Gantry signage shall be reinstated once the motorway is widened and a new gantry structure is installed.</p>	
	ATTENDED	<p>This is a long term site so the intention of the setup is to allow entry and egress without an attenuator escort. Vehicles can access the site via Rosebank Road onramp before the ramp merges with mainline traffic once they have increased their speed. A 40m clear zone applies inside the access so construction vehicles are not impeded during entry or exit maneuvers. A gate person may be used to assist entry and exit of vehicles during heavy construction periods.</p>	
	DETOUR ROUTE	<p>A detour route is not required for this TMP</p>	
	UNATTENDED	<p>Does route go into another RCA's network? <i>(If yes, has confirmation of acceptance been requested from that RCA?)</i></p>	N/A
	NIGHT WORK	<p>Any plant and/or materials shall be behind concrete barriers and kept clear of the 1m lateral safety zone and site access and exit clear zone areas. An STMS can be contacted at all times in the event of an incident on site resulting in damage to the TTM. Site accesses shall be closed with cones when not in use.</p>	
	SITE REMOVAL	<p>Any adjustments made to the site eg relocation of site access shall be completed under overnight closures under the direction of a LV2/3P STMS.</p>	
	SITE REMOVAL	<p>The site shall be removed under discreet closures of the onramp and SH16 lanes (refer to discreet TMP's for this information).</p>	

Positive Traffic Management Measures	<ul style="list-style-type: none"> • Installation of TL3 concrete barriers with 10m spaced chevrons and advance warning signage. • All truck drivers shall be briefed on the access and exit procedures by the STMS before entering site. Safe hit posts are installed for 50m after the barriers end to keep the drivers on the bus lane shoulder to get up to speed of traffic. • A gate person may be used to assist traffic entering and exiting site during heavy construction activities. • Police assistance may be sought if excess speed is a significant issue and presents a real immediate danger to the activity or the public.
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<p>Contingency Plans Generic contingencies for:</p> <ul style="list-style-type: none"> • Major incidents • incidents • pre planned detours 	<p>MAJOR INCIDENT A major incident is described as:</p> <ul style="list-style-type: none"> • Fatality or serious injury - real or potential • Significant property damage, or • Emergency services (police, fire, etc) require access or control of the site. 	<p>ACTIONS The STMS must immediately conduct the following:</p> <ul style="list-style-type: none"> • stop all activity and traffic movement • secure the site to prevent (further) injury or damage • contact the appropriate emergency authorities • render first aid if competent and able to do so • notify the RCA representative and / or the engineer under the guidance of the officer in charge of the site, reduce effects of TTM on the road or remove the activity if safe to do so • re-establish TTM and traffic movements when advised by emergency authorities that it is safe to do so
	<p>INCIDENT An incident is described as:</p> <ul style="list-style-type: none"> • excessive delays - real or potential • minor or non-inquiry accident that has the potential to affect traffic flow • structural failure of the road. 	<p>ACTIONS The STMS must immediately conduct the following:</p> <ul style="list-style-type: none"> • stop all activity and traffic movement if required • secure the site to prevent the prospect of injury or further damage • notify the RCA representative and / or the engineer • STMS to implement a plan to safely remove TTM and to establish normal traffic flow if safe to do so • re-establish TTM and traffic movements when it is safe to do so and when traffic volumes have reduced.
	<p>Note also the requirements for no interference at an accident scene: In the event of an accident involving serious harm the STMS must ensure that nothing, including TTM equipment, is removed or disturbed and any wreckage article or thing must not be disturbed or interfered with, except to:</p> <ul style="list-style-type: none"> • save a life of, prevent harm to or relieve the suffering of any person, or • to maintain the access of the general public to an essential service or utility, or • to prevent serious damage to or serious loss of property. 	
<p>Other Contingencies to be identified by the applicant <i>(i.e. steel plates to quickly cover excavations)</i></p>	<p>WEATHER In the event of inclement weather and/or at the discretion of the STMS the site is deemed to be unsafe due to limited visibility or wet conditions; work shall cease, the site made safe and all access into and out of the work area closed.</p>	
<p>Replacement crash cushions, temporary concrete barriers and site screens shall be held at the Causeway compound in the event of accident damage so replacement can occur within the stated timeframes in COPTTM.</p>		

RELEASED UNDER OFFICIAL INFORMATION ACT



Authorisations	PARKING RESTRICTION/S ALTERATION AUTHORITY	Will controlled street parking be affected?	Yes	Has approval been granted?	N/A
			No		
	AUTHORISATION TO WORK AT PERMANENT TRAFFIC SIGNAL SITES	Will portabel traffic signals be used or permanent traffic signals be changed?	Yes	Has approval been granted?	
			No		
ROAD CLOSURE AUTHORISATION/S	Will full carriageway closure continue for more than 5 minutes (or other RCA stipulated time)?	Yes	Has approval been granted?		
		No			
BUS STOP RELOCATION/S OR CLOSURE/S	Will bus stop(s) be obstructed by the activity?	Yes	Has approval been granted?	Yes	
		No			No

EED Applicable	YES		DETAILS OF EED'S ATTACHED (IF APPLICABLE)
	NO	✓	

Delay Calculations <i>(trial plan to determine extent of delays)</i>	N/A
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Public Notification Plan	N/A
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Onsite Monitoring	ATTENDED	<ul style="list-style-type: none"> The site shall be monitored by the STMS at the start and end of each shift if the TTM is permanently fixed. If there are temporary signs on stands and cones being used on site then a 2 hourly check is completed as per CoPTTM. An STMS is permitted to control all worksites for a capital project at any one time subject to the following: <ul style="list-style-type: none"> the STMS remains within 30 minutes of all worksites. that a person with a minimum qualification of STMS-NP is present and takes charge of TTM at each attended worksite. that STMS-NP must have been briefed by the STMS and the briefing documented. the STMS must be present for the set-up, alteration and removal for each of the worksites.
	UNATTENDED	<ul style="list-style-type: none"> TTM shall be installed on a permanent basis. Site accesses shall be closed with cones when not in use.
	OVERNIGHT	STMS Available 24/7
	OTHER TIMES	N/A

Method of recording daily TTM activity <i>(eg CoPTTM on-site record)</i>	Daily Job Record compiled by the STMS onsite and held as a record by Causeway Alliance
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Site safety measures	<p>All personnel working on the Project must be inducted in accordance with the Project's Safety Essentials. The Site Foreman/STMS will also conduct a pre-start meeting to identify the hazards pertaining to the work site. As a minimum, all personnel in the work site must wear a compliant high visibility long sleeved garment, a pair of safety boots, a hard hat, safety gloves and safety glasses. Visitors to the work site shall contact the STMS prior to entering the site and report to the STMS for a Site Briefing.</p> <p>During heavy construction periods and TC assistance is required a designated truck stop shall be given to the coordinator when ordering supplies. The STMS/Foreman shall coordinate trucks at the stop with the TC person via RT communication. When the TC person has received the truck inside the site he shall give the necessary directions to the driver.</p> <p>A 1m lateral safety zone maintained adjacent to all work areas (0.5m if behind concrete barriers)</p> <p>All construction vehicles entering the site must turn on their beacons before entering and exiting the work area. The beacon must be switched off once the vehicle has left site.</p>
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Reason for Revision	Gantry Structure removal and gantry signs relocated to the ground
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Site Specific Layout Diagrams	NUMBER	TITLE
	100-07 Sheets 1-5	LONG TERM SITE ACCESS PLAN SH16 E/B ROSEBANK RD-TRAHEME ISLAND SHOULDER CLOSURE WITH 80KM/HR TSL & SITE ACCESS POINT



RCA CONSENT OR CONTRACT REFERENCE	CA3695
TMP REFERENCE	100-07

Contact Details							
Principal	NZTA	NZTA Project Manager Simon Paton	[Redacted]				
	NZ TRANSPORT AGENCY WAKA KOTAHU						
Contractor	CAUSEWAY ALLIANCE	Causeway Alliance Project Manager Mark Evans	[Redacted]				
	CAUSEWAY ALLIANCE working for our thriving city						
Engineers Rep	CAUSEWAY ALLIANCE working for our thriving city	Stuart Goodes	CONTACT NO.	NZTA ID	QUAL	EXPIRY DATE	
			[Redacted]	[Redacted]	[Redacted]	[Redacted]	
STMS's	CAUSEWAY ALLIANCE working for our thriving city	NAME	CONTACT NO.	NZTA ID	QUAL	EXPIRY DATE	
			Pauline Nobbs Traffic Manager	[Redacted]	[Redacted]	[Redacted]	[Redacted]
			Nick Tapiki Primary Night STMS	[Redacted]	[Redacted]	[Redacted]	[Redacted]
			Billy Angell Primary Day STMS	[Redacted]	[Redacted]	[Redacted]	[Redacted]

TMP PREPARATION	PREPARED BY	Dave Tilton		[Redacted]	[Redacted]	[Redacted]
		Name (STMS qualified)	Signature	Qualification	ID Number	Expiry Date
CHECKED BY	Pauline Nobbs		[Redacted]	[Redacted]	[Redacted]	
	Name (STMS qualified)	Signature	Qualification	ID Number	Expiry Date	
THIS TMP MEETS COPTTM REQUIREMENTS			Number of diagrams attached: 4			

TMP RETURNED FOR CORRECTION <i>if required</i>	Name	Signature	Qualification	ID Number	Expiry Date

NOTIFICATION <i>prior to occupying worksite/ notification completed</i>	TYPE OF NOTIFICATION REQUIRED	NOTIFICATION COMPLETED	Date:
			Time:

ENGINEER/TMC <i>To complete following section when approval or acceptance required</i>	APPROVED BY TMC/ENGINEER <i>(delete one)</i>	S. GOODES		23/10/14	[Redacted]	[Redacted]
		Name	Signature	Date	Qualification	ID Number
	ACCEPTANCE BY TMC <i>(if required)</i>					
	Name	Signature	Date	Qualification	ID Number	

QUALIFIER FOR ENGINEER OR TMC APPROVAL

- This TMP is approved on the following basis:**
- To the best of the approving engineer's/TMC's judgment this TMP conforms to the requirements of CoPTTM.
 - This plan is approved on the basis that the activity, the location and the road environment have been correctly represented by the applicant. Any inaccuracy in the portrayal of this information is the responsibility of the applicant.
 - The STMS for the activity is reminded that it is the STMS's duty to postpone, cancel or modify operations due to the adverse traffic, weather or other conditions that affect the safety of this site.



DESCRIPTION	SIGN	QTY	DESCRIPTION	SIGN	QTY	DESCRIPTION	SIGN	QTY	DESCRIPTION	SIGN	QTY
Level 1 Working Man Advanced Warning			Level 2 Working Man Advanced Warning		4	Level 1 Caution Advanced Warning			Level 2 Caution Advanced Warning		
Level 2 30km/hr Ahead			Level 2 50km/hr Ahead			Level 2 70km/hr Ahead			Level 2 80km/hr Ahead		4
Level 1 On Side Road			Level 2 On Side Road			Level 1 Shoulder Closed			Level 2 Shoulder Closed		
Level 1 30km/hr			Level 2 30km/hr			Level 1 50km/hr			Level 2 50km/hr		
Level 1 60km/hr			Level 2 60km/hr			Level 2 70km/hr (900mm wide)			Level 2 70km/hr		
Level 1 80km/hr			Level 2 80km/hr		10	Level 1 100km/hr			Level 2 100km/hr		2
Level 2 Temporary (900mm wide)			Level 2 Temporary		10	Level 1 Crossing Closed			Level 1 Footpath Closed		
Level 1 detour			Level 1 Pedestrian detour right			Level 1 Site Access left		1	Level 2 Site Exit		
Barrier Chevron right pointing arrows		48	Level 1 Flagman sign			Level 1 Please stop on request			Level 2 Site Access Ahead left 400m		
Level 1 Right to Left lane shift (2 lane road)			Level 2 Right to Left lane shift (2 lane road)			Level 1 Left to Right lane shift (2 lane road)			Level 2 Left to Right lane shift (2 lane road)		
Level 2 Right to Left lane shift (4 lane road)			Level 2 Left to Right lane shift (4 lane road)			Level 2 Right to Left lane shift (3 lane road)			Level 2 Left to Right lane shift (3 lane road)		
Level 2 50m Supplementary Plate			Level 2 100m Supplementary Plate			Level 2 200m Supplementary Plate			Level 2 400m Supplementary Plate		
Level 2 Works End		2	Level 1 Exit			TL3 6m steel Barrier		223	TL3 Concrete Barrier 6m sight screen		
Level 1 Stand and base combination			Level 2 Stand and base combination		23	900mm Cone (+ 10% contingency)		58			
Stop/Go Paddle			Level 2 RG 17/34		1	End Treatment		1	Sandbags		92

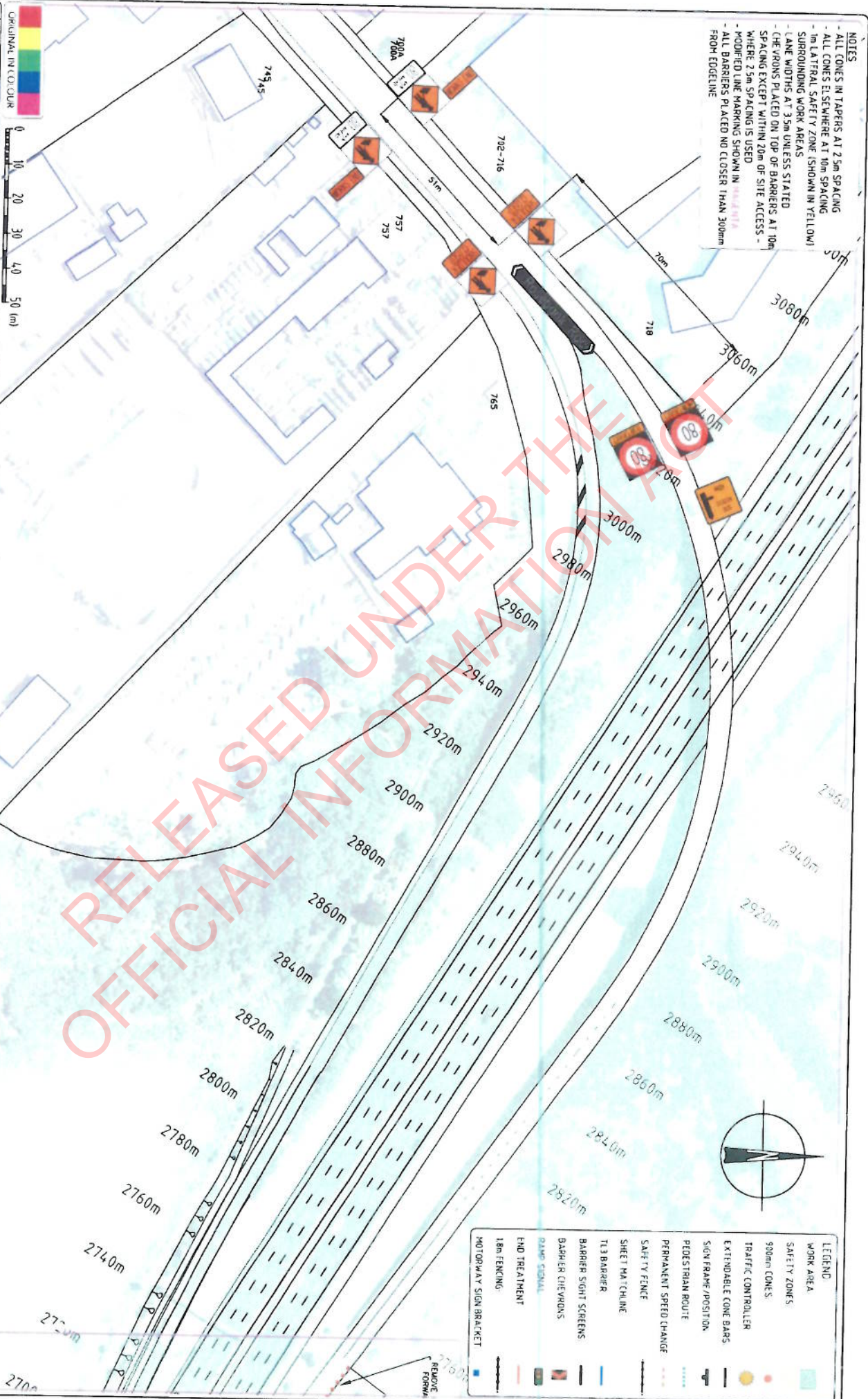
6 Gantry E2 Eastbound Situated West of Causeway bridge

Requirements; Install new temporary sign on concrete slab foundation (similar to that shown below), then remove existing gantry signage to scrap. Late Nov 14.

No TTM requirements for temporary sign but L1 and L2 Eastbound closure required for removal of existing.



- NOTES**
- ALL CONES IN TAPERS AT 2.5m SPACING
 - ALL CONES ELSEWHERE AT 10m SPACING
 - 1m LATERAL SAFETY ZONE (SHOWN IN YELLOW)
 - SURROUNDING WORK AREAS
 - LANE WIDTHS AT 3.5m UNLESS STATED
 - CHAIRONS PLACED ON TOP OF BARRIERS AT 10m SPACING EXCEPT WITHIN 20m OF SITE ACCESS - WHERE 2.5m SPACING IS USED
 - MODIFIED LINE MARKING SHOWN IN **Red**
 - ALL BARRIERS PLACED NO CLOSER THAN 300mm FROM EDGE LINE



No	Description	Date	By
1	Hold Drawing	01 FEB 2020/2014	AW
2	Revisions		

DRAWING CONSULTANT
CAUSSEY ALLIANCE
 CONSULTING ENGINEERS
 100-102 WILSON STREET, AUCKLAND
 TEL: 09 308 8888 FAX: 09 308 8889
 WWW.CAUSSEYENGINEERS.CO.NZ

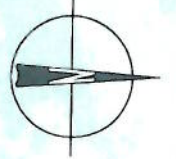
ORIGINAL SCALE
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DATE: 1 FEBRUARY 2014
 DRAWN: J. WILSON
 CHECKED: J. WILSON
 APPROVED: J. WILSON
 PROJECT: LONG TERM CONSTRUCTION
 ROSEBANK ROAD ON-RAMP
 LEVEL 3 SHOULDER CLOSURE



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**LONG TERM CONSTRUCTION
 ROSEBANK ROAD ON-RAMP
 LEVEL 3 SHOULDER CLOSURE**

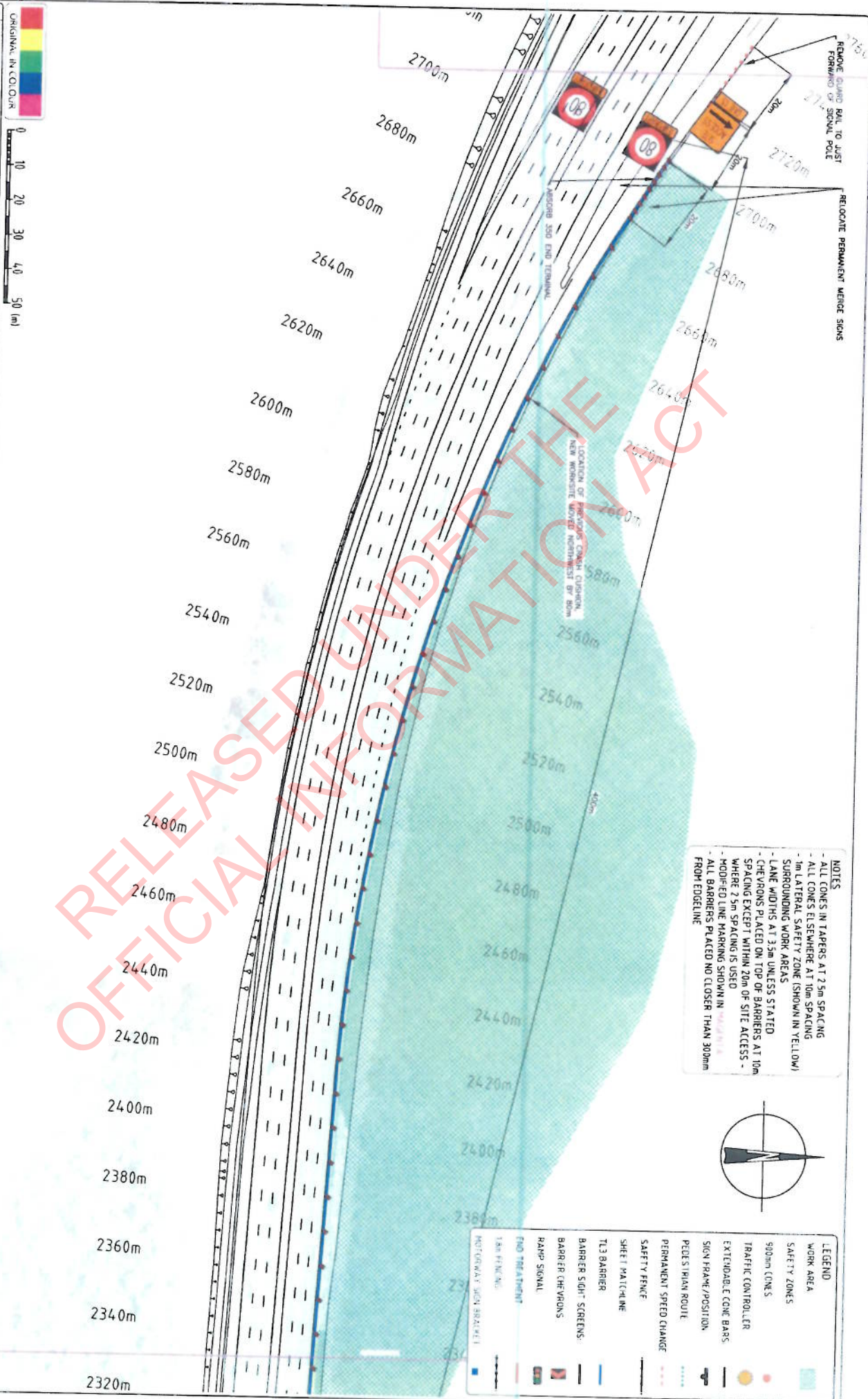
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 100-07
 SHEET NO
 Sheet 1 of 5



LEGEND

	WORK AREA
	SAFETY ZONES
	900mm CONES
	TRAFFIC CONTROLLER
	EXTENDABLE CONE BARS
	SIGN FRAME POSITION
	PIEDESTRIAN ROUTE
	PERMANENT SPEED CHANGE
	SAFETY FENCE
	SHEET MATCHLINE
	T13 BARRIER
	BARRIER SIGHT SCREENS
	BARRIER CHAIRONS
	RAMP SIGNAL
	END TREATMENT
	1.8m FENCING
	MOTORWAY SIGN BRACKET

REMOVE GUARD RAIL TO JUST FORMING OF SIGNAL POLE
RELOCATE PERMANENT MERGE SIGNS



- NOTES**
- ALL CONES IN TAPERS AT 2.5m SPACING
 - ALL CONES ELSEWHERE AT 10m SPACING
 - 1m LATERAL SAFETY ZONE (SHOWN IN YELLOW) SURROUNDING WORK AREAS
 - LANE WIDTHS AT 3.3m UNLESS STATED
 - CHEVRONS PLACED ON TOP OF BARRIERS AT 10m SPACING EXCEPT WITHIN 20m OF SITE ACCESS
 - MODIFIED LINE MARKING SHOWN IN YELLOW
 - ALL BARRIERS PLACED NO CLOSER THAN 300mm FROM EDGELINE



LEGEND

[Symbol]	WORK AREA
[Symbol]	SAFETY ZONES
[Symbol]	900mm CONES
[Symbol]	TRAFFIC CONTROLLER
[Symbol]	EXTENDABLE CONE BARS
[Symbol]	SIGN FRAME/POSITION
[Symbol]	PEDESTRIAN ROUTE
[Symbol]	PERMANENT SPEED CHANGE
[Symbol]	SAFETY FENCE
[Symbol]	SHEET MATCHLINE
[Symbol]	T13 BARRIER
[Symbol]	BARRIER SIGN SCREENS
[Symbol]	BARRIER CHEVRONS
[Symbol]	RAMP SIGNAL
[Symbol]	END TREATMENT
[Symbol]	10m FINISH
[Symbol]	NOTIFYWAY SIGN BARREL



Rev	Description	By	Doc	Date
1	Final Drawing	DL	EB	3/10/2014



ORIGINAL SCALE
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[Name]



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LONG TERM CONSTRUCTION
ROSEBANK ROAD ON-RAMP
LEVEL 3 SHOULDER CLOSURE

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SHEET NO
100-07
Sheet 2 of 5

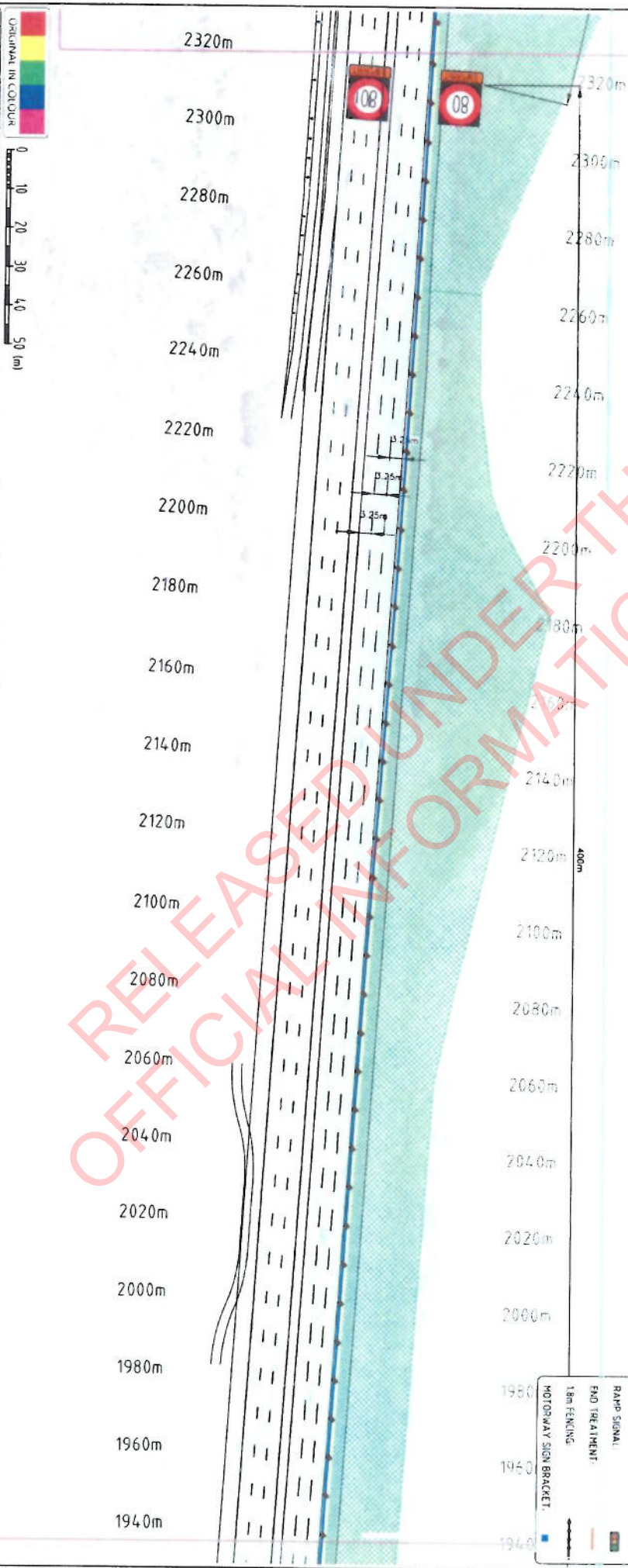


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- NOTES**
- ALL CONES IN TAPERS AT 2.5m SPACING
 - ALL CONES ELSEWHERE: AT 10m SPACING
 - 1m LATERAL SAFETY ZONE (SHOWN IN YELLOW) SURROUNDING WORK AREAS
 - LANE WIDTHS AT 3.5m UNLESS STATED
 - CHEVRONS PLACED ON TOP OF BARRIERS AT 10m SPACING EXCEPT WITHIN 20m OF SITE ACCESS - WHERE 2.5m SPACING IS USED
 - MODIFIED LINE MARKING SHOWN IN PINK
 - ALL BARRIERS PLACED NO CLOSER THAN 300mm FROM EDGE LINE



- LEGEND**
- WORK AREA
 - SAFETY ZONES
 - 900mm CONES
 - TRAFFIC CONTROLLER
 - EXTENDABLE CONE BARS
 - SIGN FRAME/POSITION
 - PEDESTRIAN ROUTE
 - PERMANENT SPEED CHANGE
 - SAFETY FENCE
 - SHEET MATCH LINE
 - T1.3 BARRIER
 - BARRIER SIGHT SCREENS
 - BARRIER CHEVRONS
 - RAMP SIGNAL
 - END TREATMENT
 - 1.8m FENCING
 - HORIZONTAL SIGN BRACKET



REV	DESCRIPTION	BY	DATE	ORIGINATOR
1	ROAD DESIGN	DL	15/02/2014	CAUSEWAY ALLIANCE Working for our customers

ORIGINAL SCALE
A5

DRAWN & CHECKED BY
Derek L. [unreadable]
15/02/2014

PREPARED FOR CLIENT / CONTRACTOR / PROJECT / CONTRACT
NZ TRANSPORT AGENCY
WAIKA KOTAHU

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LONG TERM CONSTRUCTION
ROSEBANK ROAD ON-RAMP
LEVEL 3 SHOULDER CLOSURE

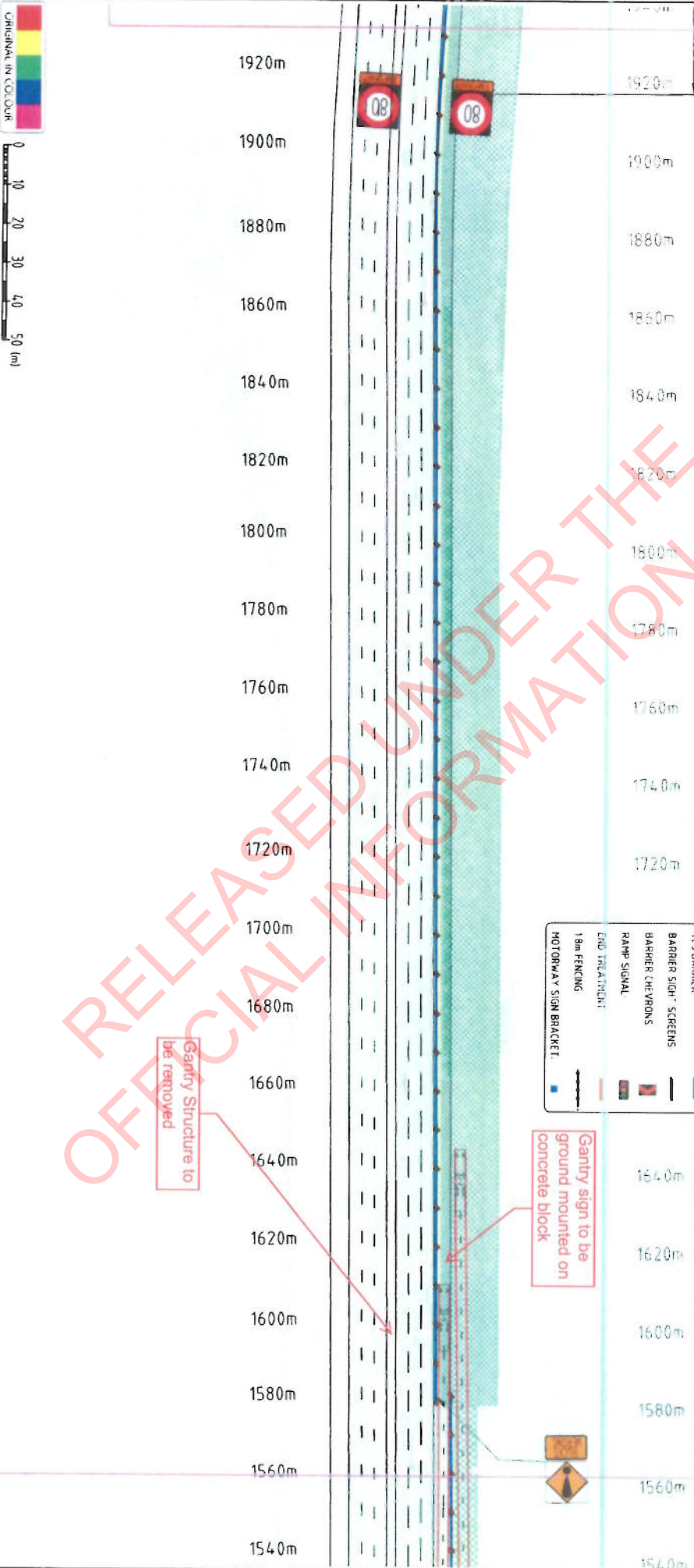
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SHEET NO
Sheet 3 of 5

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- NOTES**
- ALL CONES IN TAPERS AT 2.5m SPACING
 - ALL CONES ELSEWHERE AT 10m SPACING
 - 1m LATERAL SAFETY ZONE (SHOWN IN YELLOW)
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 - ALL BARRIERS PLACED NO CLOSER THAN 300mm FROM EDGE LINE

- LEGEND**
- WORK AREA
 - SAFETY ZONES
 - 900mm CONES
 - TRAFFIC CONTROLLER
 - EXTENDABLE CONE BARS
 - SIGN FRAME/POSITION
 - PEDESTRIAN ROUTE
 - PERMANENT SPEED CHANGE
 - SAFETY FENCE
 - SHEET MATCHLINE
 - T13 BARRIER
 - BARRIER SIGN SCREENS
 - BARRIER CHEVRONS
 - RAMP SIGNAL
 - END TREATMENT
 - 1.8m FENCING
 - MOTORWAY SIGN BRACKET



Rev	Description	By	Date
1	Issue Drawing	DL	18/02/2017
2			
3			

DRAWING ORGANISATION

CAUSEWAY ALLIANCE
Working for our clients

ORIGINAL SCALE

1:1000

DRAWN & CHECKED BY

Drawn by: [Name]
 Checked by: [Name]



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LONG TERM CONSTRUCTION
ROSEBANK ROAD ON-RAMP
LEVEL 3 SHOULDER CLOSURE

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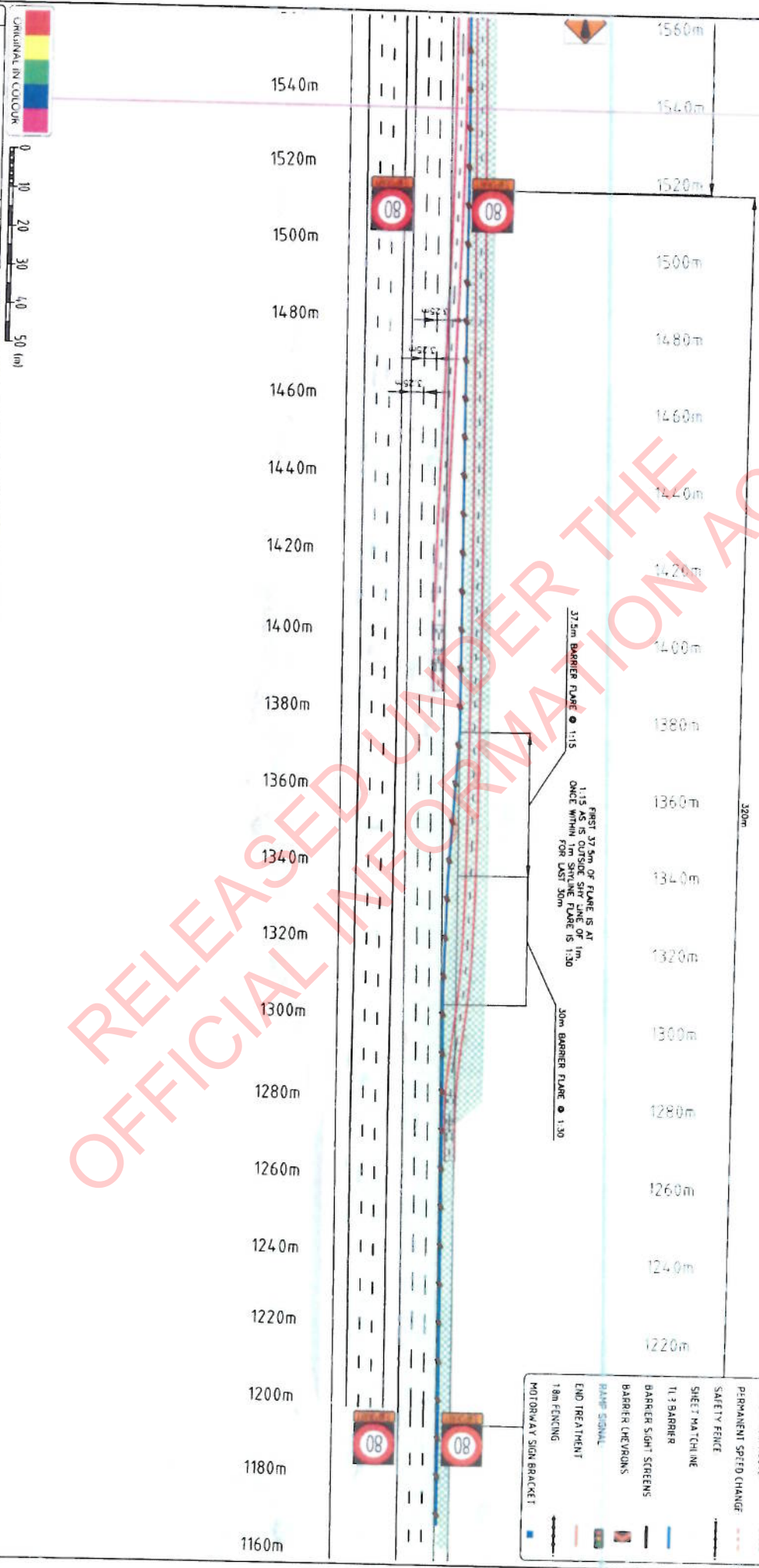
SHEET NO
 Sheet 4 of 5



- NOTES**
- ALL CONES IN TAPERS AT 2.5m SPACING
 - ALL CONES ELSEWHERE AT 10m SPACING
 - 1m LATERAL SAFETY ZONE (SHOWN IN YELLOW) SURROUNDING WORK AREAS
 - LANE WIDTHS AT 3.5m UNLESS STATED
 - CHEVRONS PLACED ON TOP OF BARRIERS AT 10m SPACING EXCEPT WITHIN 20m OF SITE ACCESS - WHERE 2.5m SPACING IS USED
 - MODIFIED LINE PARKING SHOWN IN **RED/WHITE**
 - ALL BARRIERS PLACED NO CLOSER THAN 300mm FROM EDGE LINE



- LEGEND**
- WORK AREA
 - SAFETY ZONES
 - 900mm CONES
 - TRAFFIC CONTROLLER
 - EXTENDABLE CONE BARS
 - SIGN FRAME/POSITION
 - PEDESTRIAN ROUTE
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 - BARRIER CHEVRONS
 - RAMP SIGNAL
 - END TREATMENT
 - 1.8m FENCING
 - MOTORWAY SIGN BRACKET



Rev	Description	By	CHK	DATE
1	Issue Drawing	DL	EB	10/2/2014
2				
3				



PROJ. SCALE
1:1000

DESIGNED BY
DANIEL E. CRITCHFIELD



DRAWING TITLE
LONG TERM CONSTRUCTION
ROSEBANK ROAD ON-RAMP
LEVEL 3 SHOULDER CLOSURE

DRAWING TITLE
100-07
SHEET NO
Sheet 5 of 5



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