

## MANAGEMENT PLAN

Provide an outline of:

- The project management process to be followed
- Arrangements/structures for the governance and management of the project, how these are related, and the experience and qualifications of key individuals involved
- The ownership/management of the end result of the project
- How post-project evaluation will be carried out

## NEXT STEPS

Outline the immediate steps to be taken in the first three to six months if funding is approved.

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**From:** [Mark Patterson](#)  
**To:** [Julia Paterson-Fourie](#)  
**Subject:** FW: Business case drafts [IN-CONFIDENCE]  
**Date:** Wednesday, 14 March 2018 9:46:00 a.m.  
**Attachments:** Out of Scope [REDACTED]

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**From:** Eliot Linforth-Hall  
**Sent:** Tuesday, 13 March 2018 5:07 p.m.  
**To:** Andy Nock; Mark Patterson; Stephanie Weller  
**Subject:** Business case drafts [IN-CONFIDENCE]

Hi Andy,

Please find attached the three business case drafts we've been working on with you. Please let me know if you don't have a copy of the Russell wharf business case that you and Mark worked on. I spoke briefly to Lawrence about the CBA. I hope you have been able to talk to him and organise something that you are happy with. The three wharfs will also require some CBA work. If you have any queries, please contact Mark or myself.

**Eliot Linforth-Hall**  
Project Manager  
Regional Economic Development  
Ministry of Business, Innovation and Employment  
Mobile <sup>s 9(2)(a)</sup> [REDACTED] [Eliot.Linforth-Hall@mbie.govt.nz](mailto:Eliot.Linforth-Hall@mbie.govt.nz)  
15 Stout Street, Wellington 6011 | PO Box 1473 Wellington 6140

 **MINISTRY OF BUSINESS,  
INNOVATION & EMPLOYMENT**  
HIKINA WHAKATUTUKI



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**From:** Byron Elliot (Byron)  
**To:** [Mark Patterson](#); [Sione Tuiata-Alofi](#); [Tony de Jong \(Tony\)](#)  
**Subject:** FW: MBIE Funding Applications - Draft  
**Date:** Thursday, 8 February 2018 10:23:13 a.m.  
**Attachments:** [3b. Russell Wharf Business case templates \(RGI\).docx](#)  
Out of Scope

**Importance:** High

FYI only – advance copy. Comments are being worked on and they are aiming to send me something more solid after lunch.

**From:** § 9(2)(a)  
**Sent:** Thursday, 8 February 2018 10:06 AM  
**To:** Byron Elliot (Byron) <Byron.Elliot@mpi.govt.nz>  
**Subject:** FW: MBIE Funding Applications - Draft  
**Importance:** High

Russell Wharf

Out of Scope

**From:** Kylie Cox [[mailto:§ 9\(2\)\(a\)](mailto:§ 9(2)(a))]  
**Sent:** Wednesday, 7 February 2018 4:59 p.m.  
**To:** [jude.thompson@northlandnz.com](mailto:jude.thompson@northlandnz.com); § 9(2)(a)  
**Cc:** Andy Nock <§ 9(2)(a)>  
**Subject:** MBIE Funding Applications - Draft  
**Importance:** High

Good Afternoon Ladies,

Andy has asked me to forward the attached DRAFT documents to you for your review and comment. If we could please have any comments back to both Andy and myself, **no later than tomorrow, midday Thursday 8 February 2018**, in order to meet the funding deadlines.

I appreciate the quick turn around.

I have included the 3 Business Case Templates with track changes to make this easier if you prefer to add your comments directly to the document, otherwise list the suggestions via return email.

Kind Regards

Kylie Cox  
Executive Assistant  
Far North Holdings Limited  
PO Box 7

OPUA, 0200

s.9(2)(a)

[www.fnhi.co.nz](http://www.fnhi.co.nz)

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**From:** [Mark Patterson](#)  
**To:** [Iain Southall](#); [Andrew Beaufort](#); [John Doorbar](#)  
**Subject:** FW: Minister Jones Northland visit - actions [IN-CONFIDENCE:RELEASE EXTERNAL]  
**Date:** Monday, 5 February 2018 2:59:00 p.m.

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FYI

Please note Minister met with Far North Holdings on Friday and wants to progress the below highlighted.

Cheers

Mark

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**From:** Byron Elliot (Byron) [mailto:Byron.Elliot@mpi.govt.nz]  
**Sent:** Friday, 2 February 2018 7:07 p.m.  
**To:** Grant Berghan; Jane Frances; Ben Dalton; Tony de Jong (Tony); Luke Southorn (Luke); Mark Patterson; Stephanie Weller  
**Subject:** Minister Jones Northland visit - actions

Hi all,

Here are the actions that I noted down today (I sure I have missed some – feel free to add/change). Will have a look back through my notes and will follow up on Monday.

Note there are some actions for the 23 Feb launch.

Thanks all – have a good weekend.

Cheers,

Byron

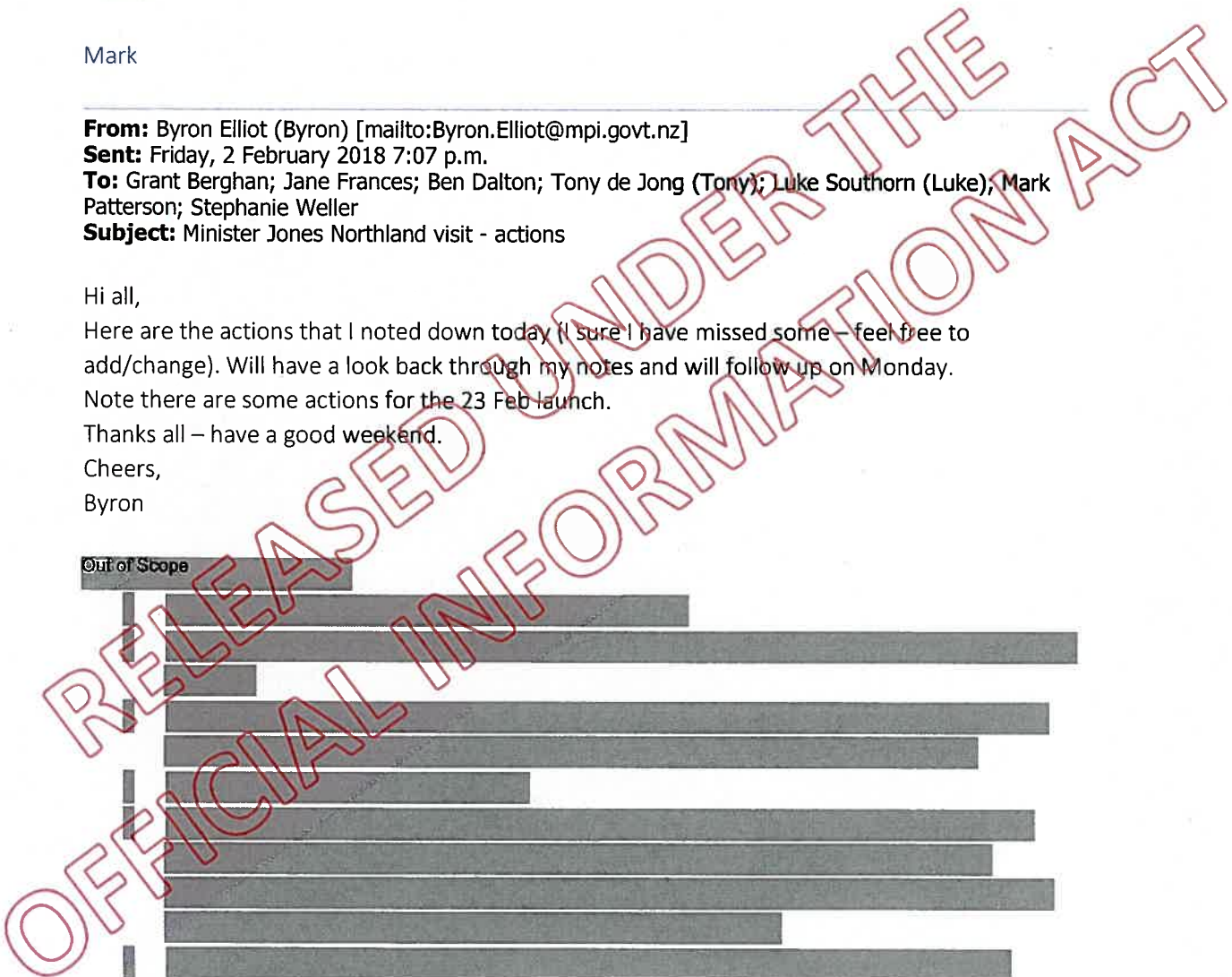
Out of Scope

[Redacted content]

[Redacted content]

**Andy Nock – Far North Holdings**

- Pitched **Out of Scope**
- **Russell Wharf**, **Out of Scope**



- Funding gap - \$21.945m
- **ACTION:** Andy Nock will send through information to inform a "tourism package" for 23 Feb announcement which will include:
  - Wharf package – Out of Scope Russell Out of Scope )
  - Out of Scope
- The other proposals may form a pipeline for July – Minister to test with other Ministers.
- **ACTION:** MBIE/MPI to discuss capacity to deliver this for 23 Feb announcement.  
(Byron/Mark)

Out of Scope

[Redacted]

[Redacted]

[Redacted]

[Redacted]

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[Redacted]

Out of Scope

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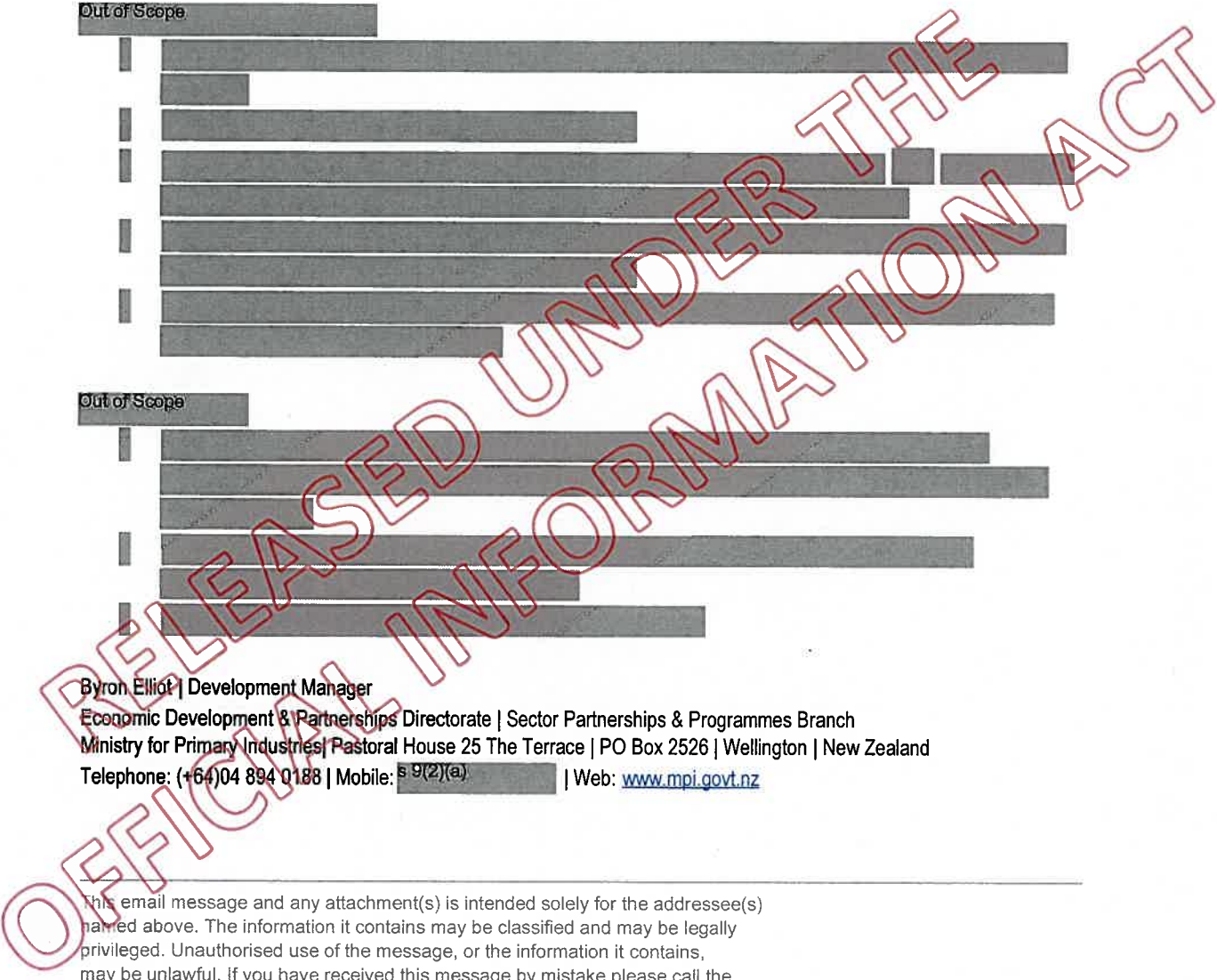
[Redacted]

[Redacted]

Byron Elliot | Development Manager  
 Economic Development & Partnerships Directorate | Sector Partnerships & Programmes Branch  
 Ministry for Primary Industries | Pastoral House 25 The Terrace | PO Box 2526 | Wellington | New Zealand  
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**From:** Byron Elliot (Byron)  
**To:** [Tony de Jong \(Tony\)](#); [Mark Patterson](#); [Sione Tuita-Alofi](#); [Anna McGuinness](#)  
**Subject:** FW: Ruesell Wharf Business Case  
**Date:** Thursday, 8 February 2018 4:32:41 p.m.  
**Attachments:** [Final Russell Wharf Business case .docx](#)  
[Final Russell Wharf Business case .pdf](#)

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FYI – one more to come

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**From:** Kylie Cox [mailto:[s.9\(2\)\(a\)](#)]  
**Sent:** Thursday, 8 February 2018 4:27 PM  
**To:** Byron Elliot (Byron) <Byron.Elliot@mpi.govt.nz>  
**Cc:** Andy Nock [[s.9\(2\)\(a\)](#)]  
**Subject:** Ruesell Wharf Business Case

Hi Byron,

Please see attached doc's for the Russell Wharf

Kind Regards

Kylie Cox  
Executive Assistant  
Far North Holdings Limited  
PO Box 7  
OPUA, 0200  
[s.9\(2\)\(a\)](#)  
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**From:** [Mark Patterson](#)  
**To:** [Jared Humm](#)  
**Subject:** FW: Russell Wharf Business Feedback [IN-CONFIDENCE:RELEASE EXTERNAL]  
**Date:** Tuesday, 6 March 2018 11:05:00 a.m.  
**Attachments:** [Final Russell Wharf Business - MBIE Co.docx](#)

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**From:** Andy Nock [mailto:[s 9\(2\)\(a\)](#)]  
**Sent:** Tuesday, 6 March 2018 9:16 a.m.  
**To:** Mark Patterson  
**Subject:** RE: Russell Wharf Business Feedback [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi Mark,

I apologise for the delay in responding but have been away the past week on vacation and only returned today.

Attached is the Russell Wharf Business Case with my comments.

In my absence the QS completed a review of the development Plan for Russell Wharf. The functionality and design layout were reviewed in light of recent boat impact damage to pontoon piles. The main finding of the review was the need to correct the absence of fendering for the main service pontoon. Fender piles will protect the pontoon piles from damage from contact with vessels and prolong the life of the pontoon structure as a whole. I would like the inclusion of these two piles in the development plan at \$48,000 each (\$96,000) plus GST and have amended the cost plan to reflect this. If there is any issue with adding this, at this late stage, please advise me.

Out of Scope

I am in Wellington Friday and available to meet in the morning if you are about?

Cheers,

Andy

**Andy Nock**

**Chief Executive**  
**Far North Holdings Limited**  
PO Box 7  
Opua 0241  
[s 9\(2\)\(a\)](#)

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**From:** Mark Patterson [<mailto:Mark.Patterson2@mbie.govt.nz>]

**Sent:** Friday, 2 March 2018 4:44 p.m.

**To:** Andy Nock [s.9\(2\)\(a\)](mailto:s.9(2)(a)@mbie.govt.nz)

**Subject:** Russell Wharf Business Feedback [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi Andy,

Apologies this has taken some time.

We now have some time to redraft the business cases so they are more robust and meet our full requirements for funding.

I have completed feedback on one of the Russell Wharf proposal, however I assume feedback would spill over to the other wharf proposals. [Out of Scope](#)

Can I ask that your team work on getting more information into this document, and provide any other supporting information to supplement it (i.e. the plans, images etc.)

Happy to work with whomever to get these up and done ☺

Cheers

Mark

**Mark Patterson**  
SENIOR ADVISOR  
Regions and Cities  
Tourism, Sectors, Regions and Cities  
Labour, Science & Enterprise Group

**Ministry of Business, Innovation & Employment | Te Manatu Pakihi, Auahatanga me Nga Kaupapa a-Mahi**

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## **Regional Growth Initiatives Multi Year Appropriation**

### **3b. Business Case Templates**

April 2017

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## Regional Growth Initiatives Multi Year Appropriation Business Case

### Russell Wharf

Regional Lead/Applicant

Prepared by	Andy Nock, of Far North Holdings Ltd
Prepared for	MBIE
Date	08.02.2018
Version	1

### Document Control

Document ID		
File name	Russell Wharf MBIE Business Case	
Version Number	Issue date	Changes/actions
1	08.02.2018	

### Document sign-off

Name	Role	Sign-off date

## Checklist and other annexes

### COMPLETED

Executive summary	
Strategic case	
Economic evaluation	
Project plan	
<i>Operational budget</i>	
Management plan	
Next steps	

You should also attach any supporting documents. This must include evidence of endorsement by the regional lead which will be responsible for the relevant project, and could also include letters of support from regional stakeholders, governance documents, designs/concept development, feasibility studies, economic or risk evaluations or any document which supports assumptions, measurements or judgements made in the business case. Please list these in order below, and reference each document.

	Document (title)	Purpose
1	<b>Strategic Context</b>	<b>To provide an overview of how the five projects integrate within the district</b>
2	<b>Wharf plan &amp; i-SITE drawings</b>	<b>To identify the proposed development</b>
3	<b>QS estimate</b>	<b>Provide an accurate estimation of likely tender submission based on current construction costs</b>
5	<b>Letters of Support</b>	<b>Show the support for the development as proposed</b>
6	<b>ME Economics</b>	<b>High level case review</b>
7	<b>Statistical data and press article</b>	<b>Economic benefit from cruise ship passengers and actual statistical data</b>

## EXECUTIVE SUMMARY

- Russell Wharf serves as the community's connection to the rest of New Zealand. It is the most important piece of community and tourism infrastructure.
- Historical development of the wharf has been funded by; FNDC, FNHL and the community.
- There is an active Wharf Trust that provides support and guidance on the wharf's development and maintenance.
- Approximately 850,000 passengers use the wharfs ferry transport and the commercial tourism services that operate from here.
- Russell is a visitor highlight in the Bay of Islands. It has a rich maritime history. It hosts several nautical events including:
  - Coastal Classic
  - Millennium Superyacht Cup
  - Russell Birdman
  - School swimming sports
  - Several sports fishing events
  - Cruise Ship tenders
  - Ocean Swim
- Part of the existing infrastructure is built around the original old timber piles and provides low tidal landings which are unsafe and provide low utility value.
- FNHL are currently in the process of replacing the main commercial pontoon P5, which recently failed, and carrying out an expansion of the information kiosk and adding to this a café facility. The fuel jetty has also recently been replaced with a new fuel pontoon.
- The proposed new development has been endorsed by the local Wharf and Maritime Trust and includes:
  - Replacing the low tidal landings with floating concrete pontoons.
  - Removing the fixed timber landing jetty and replacing this with a concrete pontoon.
  - New dinghy dock.
  - Wharf extension to the west to provide more visitor space and improve passenger flow.
  - Jumping platform (controlled)
  - Sewage and water services across the fuel pontoon.
  - 4 super yacht mooring blocks.
- Developments are permitted as replacing existing structures. Wharf extension and dinghy dock will need consent, and this can be readily obtained based on prior consultation work.
- Cost to develop as per the engineers cost plan is \$1,114,000 this excludes the \$900,000 being invested by FNDC and FNHL to replace the front commercial pontoon and to redevelop the i-SITE and café building to include public toilets and increased circulation area around the building.

## STRATEGIC CASE

### Investment objectives

<b><i>Project Objective One</i></b>	<b>ENSURE THAT - RUSSELL WHARF AS A KEY PIECE OF DISTRICT INFRASTRUCTURE THAT UNDERPINS THE TOURISM ECONOMY OF NORTHLAND IS FIT FOR PURPOSE</b>
Existing arrangement	Russell wharf was constructed in 1970 and was not designed for the commercial fleet that now operate from the wharf. Furthermore, it was not designed for the number of customers that now utilise the wharf facilities and board the charter vessels, and cruise ship tenders that operate from here. In addition, the i-SITE is small and constrained so is to be redeveloped and extended providing increased local promotion, café and public toilets.
Business need/scope	
How will the project meet this need?	Russell wharf has been redesigned and reconfigured to allow wider and more customer friendly circulation and waiting areas, in addition we have created an additional number of berth faces to meet the changed needs of the modern commercial fleet that operate here and cater for the increased number of boats wishing to operate from the wharf. A new i-SITE and café will support the customers visiting Russell and provide new and improved wharf facilities.

<b><i>Project Objective Two</i></b>	<b>ENSURE THE WHARF CAN MEET THE CURRENT NEEDS OF THE MARITIME CHARTER FLEET AND PROVIDE A CUSTOMER VISITOR EXPERIENCE</b>
Existing arrangement	Russell wharf was constructed in 1970 and was not designed for the commercial fleet that now operate from the wharf. Furthermore, it was not designed for the number of customers that now utilise the wharf facilities and board the charter vessel, and cruise ship tenders that operate from here.
Business need/scope	
How will the project meet this need?	The new berth faces have been designed to meet the new boats that have been constructed by both, Explore Ltd and Intercity Group Ltd (Fullers). A new cruise ship tender pontoon has been proposed to provide a tender facility for the increasing number of anchoring cruise ships out in the Bay. Furthermore, additional berth space has been provided for the growing charter fleet that operate and provide a variety of experiences within the Bay.



<b>Project Objective Three</b>	<b>PROVIDE FOR A FUTURE PROOFED PAIHIA WHARF THAT WILL NOT ONLY MEET THE CURRENT NEEDS AND THAT IS ALSO DESIGNED TO ALLOW FOR FURTHER GROWTH AND HAS BEEN SPECIFIED SO IS TO MEET THE POTENTIAL CHANGING REQUIREMENTS OF THE EXISTING FLEET</b>
Existing arrangement	Russell wharf was constructed in 1970 and was not designed for the commercial fleet that now operate from the wharf. Furthermore, it was not designed for the number of customers that now utilise the wharf facilities and board the charter vessel, and cruise ship tenders that operate from here.
Business need/scope	
How will the project meet this need?	Additional berth faces are being provided over and above that currently required, reflecting the projected growth in demand, and in addition, the pontoons are being designed to cater for a range of vessels that berth against the pontoon face. We are ensuring pontoon height, for boarding and disembarking, gangway access etc will meet all user needs.

<b>Project Objective Four</b>	<b>TO IMPROVE THE ENVIRONMENTAL OUTCOME BY PROVIDING PUMP OUT FACILITIES</b>
Existing arrangement	None exist.
Business need/scope	
How will the project meet this need?	By providing a new pump out berth which will encourage pump out rather than discharge within the Bay.

Key strategic risks

Risk	Responsible party	Risk treatment (by applicant)
Resource Consent	FNHL	Consent application has been lodged, but is only a variation to existing consents held by FNHL and is likely to be dealt with under discretionary authority. Risk is minimal.
Commercial charter users and public support	FNHL	18 months of consultation has already taken place with commercial user groups. The design reflects their input, letters of support are attached.
Project does not come with-in the QS estimate when tendered	FNHL	FNHL may need to value engineer the project, if on tender the tenders received exceed budget.
No contractor tenders for the work	FNHL	Project delayed and re-tendered later. Risk minimal. At this stage we have pre-qualified 2 contractors whom are both available and intimated they would tender for the project.

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## High level objectives alignment

Stakeholder	Relevant high-level objective(s)	Explain contribution/alignment
Northland Inc	(1) Align with the Northland Economic Action Plan	Delivering to this as an identified key project.
Far North District Council	(1) Key piece of district infrastructure	By providing assurance that the wharf will exist and be bought up to a specification that will continue to underpin the tourism economy of Northland.
Northland Regional Council	(1) Environmental Protection	Providing pump out facilities to reduce marine pollution within the Bay.
Commercial users of the wharf	(1) New I-SITE & café building	New i-SITE and café will provide waiting customers premises they can wait for the ferry, get refreshments and make enquiries.
	(2) Improving customer and pedestrian circulation and berth waiting areas	A new larger deck outside the café and gangway to the ferry will provide improved customers flow and satisfaction and therefore results in increased pedestrian use on the wharf and allows further growth.
	(3) Increased number of berth facilities	Allows for further growth of the existing commercial fleet that operate from the wharf, thereby increasing investment; employment and growth of Russell and its surrounds.
Various	(1) Employment	Construction employment, and apprenticeships employed over the construction period under the three contracts, Out of Russell Wharf Out of Commercial users; by developing Russell Wharf, the existing fleet, which has invested considerably on vessels over the past couple of years will be able to continue this growth with the result in employment by each user, and the flow on effect is then into the surrounding economy with additional persons staying at hotels, restaurants and the retail which surround and rely on the wharf for customers to stay.
	(2) Pipeline to increased employment and training	Increased tourism numbers will result in an increase in employment and training opportunities across the far north within primarily the tourism sector but with flow on to other core industries.

## ECONOMIC EVALUATION

### Cost/benefit breakdown

	<b>PREFERRED OPTION</b> <i>Requested investment</i>	<i>EXISTING SITUATION</i>
Period of expected economic benefits from project (years)	In perpetuity	Limited life, with a likelihood that the wharf will be compromised if investment is not made in the immediate future. Several berths and pontoons are nearing the end of their life and will start to fail, resulting in their removal or decommissioning
<i>Capital/whole of life costs</i>	\$1,114,000	50-year life
<b>Cost-benefit analysis of monetary costs and benefits</b>		
Present value of monetary benefits		
Present value of costs	\$1,114,000	
Net present value		
Benefit/cost ratio		

## PROJECT PLAN

<b>Outline the procurement process used/to be followed</b>
Contract to be tendered NZS3910.
<b>Outline the key project requirements, used/to be used in procurement</b>
A tender to be placed to the open market and a normal tender procedure would follow. An analysis of the tender will be made in terms of: contractor, availability, price, quality etc. FNHL will provide full project management services, act as engineer to the contract and will ensure contractor payment certificates are validated throughout the process to ensure both, security, accountability and delivery of the contact on budget and time.

### Project timeline

DATE	Project milestone	Associated payment	Evidence/reporting required
08/02/2018	Building Consent / Resource Consent Submitted		
30/04/2018	Building Consent / Resource Consent received & construction contract awarded		
31/05/2018	Tender construction programme		
20/12/2018	Completion date		

### Key project risks

Risk	Responsible party	Risk treatment (by applicant)
Variations to Contract	FNHL	No variations are envisaged. The contract will be prescriptive, and a fixed price contract will be sought.
Weather	FNHL	Unavoidable, whilst this may delay the delivery date of the contract, this should not increase the value of the contract unless the bad weather days exceed the time allowed for within the final agreed construction contract that has been executed.
If the summer trading season is impacted because of the contractor not being available for the programmed development period then the construction contract may need to be split over two seasons.	FNHL	Contract management by Far North Holdings Limited.

Operating budget

Applicant and project name					
Preferred option	Year 0	Year 1	Year ...	...	Total
<u>Expenditure</u>					
<u>Capital expenditure</u>	(b) (2)(b) (1)				(b) (2)(b) (1)
<u>Contingency</u>					
TOTAL					
<u>Operating expenditure</u>					
TOTAL					
<u>Co-funding secured, source</u>		FNHL/PNDC			
TOTAL					
Capital funding required	\$1,114,000				\$1,114,000
Operating funding required	\$0.00				\$0.00
Funding shortfall (if any)	\$1,114,000				\$1,114,000

## MANAGEMENT PLAN

Far North Holdings Limited is the Far North District Council's commercial infrastructure company. Far North Holdings Limited involvement reflects the commitment of the Far North District Council to supporting the development of his part of the district.

FNHL are Certified International Port Security Accredited.

Far North Holdings Limited will provide project management, value engineer the project through the development cycle and acting as engineer to the contract.

## NEXT STEPS

If funding is approved, Consent will be finalised, and tenders sought.

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## Strategic Context

Tourism is now New Zealand's top export which has brought capacity issues in some regions. As outlined in Tourism 2025, New Zealand's National Tourism Strategy, providing alternative visitor options in other regions can assist dispersal of visitors to ease this pressure. Auckland Airport and Air New Zealand have expressed support for improving the number, quality and dispersal of experiences in Northland to provide another option for short stay visitors coming through Auckland.

Visitor numbers and tourism spend in the Northland region are currently growing at almost double the national average. The Far North's ability to maximise the potential of this growth is hindered because it lacks sufficient quality tourism experiences that would attract the growing visitor numbers to New Zealand.

Despite recent increases in employment in the regions, Northland is still lagging other regions. The Far North has the third lowest employment rate of all New Zealand districts. Visible employment opportunities are needed to give rangatahi a reason to complete their compulsory education and to give them hope for a better future. Northland has the second highest Maori population of any New Zealand region - any opportunities for tangata whenua to remain in their rohe and connected to their cultural roots will strengthen the community and help the region to maximise its economic potential.

Northland is currently New Zealand's 6<sup>th</sup> largest tourism region, measured in both guest nights and spend. The Tai Tokerau Northland Regional Economic Action Plan prioritises developing more compelling value propositions based on strengthening our existing infrastructure, linking cultural and natural advantages and creating authentic visitor experiences throughout the region which ultimately encourage the visitor to stay longer; reducing the seasonality of the sector through all-weather tourism products; and improving product dispersal across the region.

Locally the environment in which a development like this exists is also changing:

- Waitangi Trust promoting a 200-bedroom 5-star Hotel;
- The potential development of a new National icon – the Maori Battalion Museum at Waitangi
- The Peppers Carrington Resort (on the Karikari Peninsula) once fully developed (700 additional beds), both will be demanding experiences to meet the needs of its guests;
- More critically the development of one or more of the above, will create significant additional pressure on the airport with potentially 2 flights operating simultaneously to cater for arriving tourists connecting through Auckland;
- Redevelopment of Te Waiariki Ngawha Springs to include continued improvements to the visitor experience at the pools; addition of products and or experiences such as nature walks; recreational swimming facilities; spa products and development of a hauora wellness component;
- A potential 50 bed Resort nearby Te Waiariki Ngawha Springs and high-end spa facility that caters to a higher end tourist
- Cruise ship numbers to the Bay of Islands have increased, increasing pressure on the wharf and if you add to this the increased tourism numbers generated simply by the above developments, and do not allow for any additional growth in the Bay of Islands, the wharf structures cannot cope with this level of foot traffic or the increased boat number seeking to meet this demand for excursions; and
- The QRC Tai Tokerau Resort Training College at Paihia is providing training to support the development of high quality customer/tourism staffing.

Strategically, the airport and wharfs underpin the Far North District. The success of the Action Plan's most recent addition, the Tier One legacy projects and programme 'Tuia – Encounters 250' relies



heavily on improved infrastructure to maximise the experiences of acknowledging the early meetings of Māori and Europeans when James Cook and Tahitian chief and navigator Tupaia arrived in 1769.

All other attractions “feed” of these and rely on their very existence, as reflected in the number of people that pass across these.

Improving these will ensure that Northland continues to maintain its position as a tourist destination and allow it to continue to grow its market share of both domestic and International tourism numbers.

## Employment

The Airport will see Air NZ, the Hire Car Companies and Café recruiting additional staff estimated at 7 FTE's.

The Wharf structures if re-built will allow all the existing charter businesses to continue their investment in additional and larger boats. This in turn increases the employees of each operating entity. It then has the flow on affect by supporting more visitor overnight stays in Paihia and Russell and so creates opportunity in the hotel, restaurant and retail sectors that rely on these activities being available for their paying customers.

The Te Hononga Centre will provide local employment opportunities in an area of high unemployment.

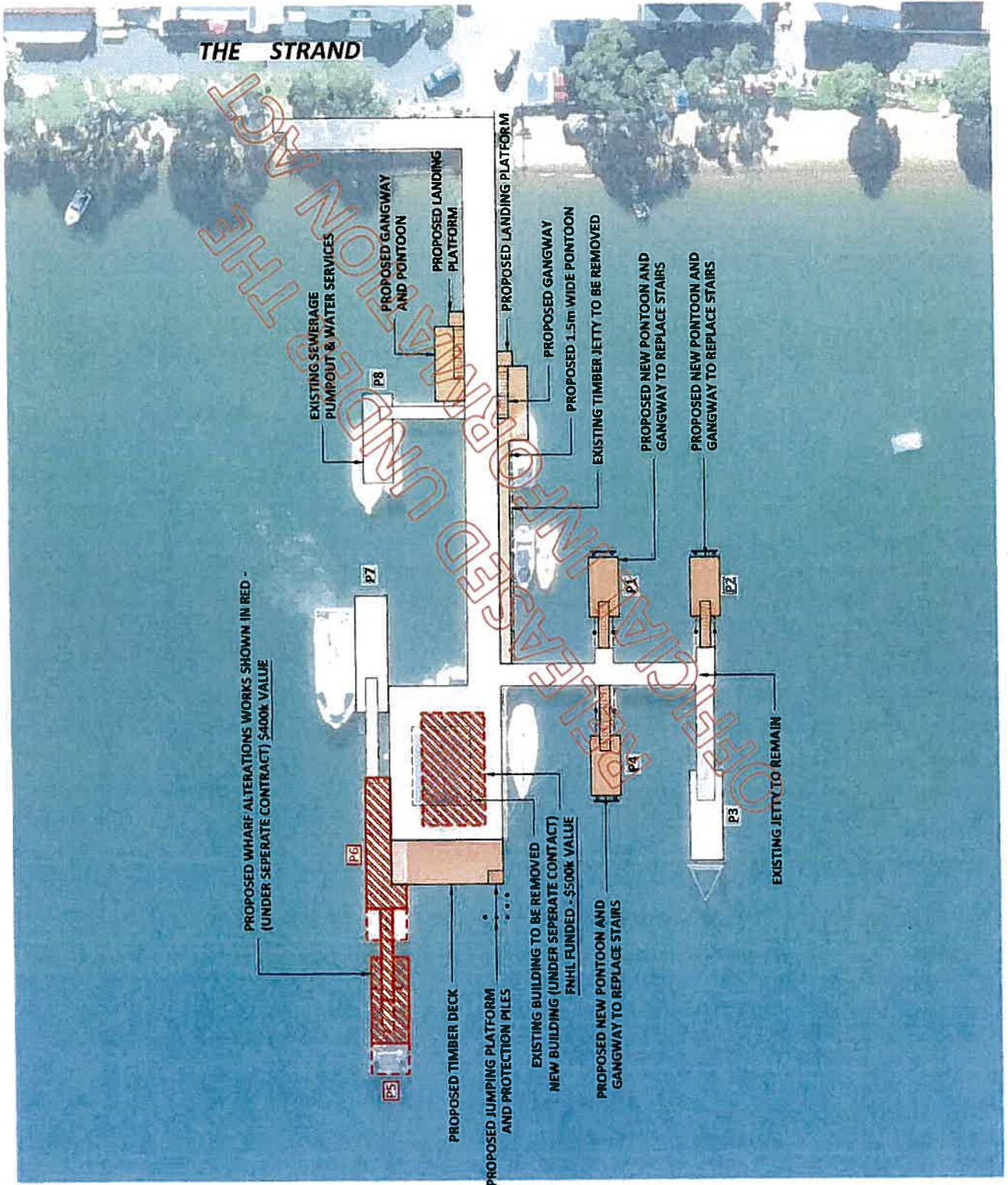
An estimated 5 FTE's will be employed as a direct consequence of the new interpretive centre and work shop and 10 volunteers will be introduced on opening. TRC estimate \$1,750,000 will flow annually into the local economy. (Assuming a local business generates approx. \$400,000 per annum this is the equivalent of 4 new businesses being supported or an additional 8 FTE's creating a net 13 FTE's).

This Centre will provide a source of trained staff for other local businesses as they grow and can provide support for higher quality visitor services.

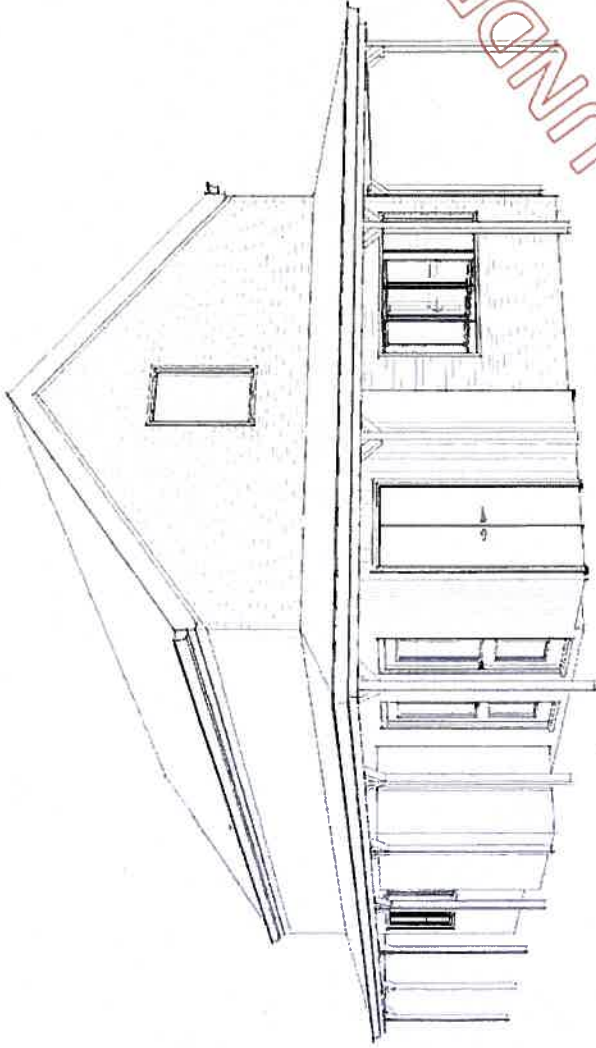
Impact for future employment – the Centre and other businesses that grow up around it will give the rangatahi a tangible reason to complete their education through:

- providing opportunities for employment in the creative arts (storytelling, powhiri and digital presentation) that is often more appealing to them;
- providing opportunities for rangatahi to have hands on training in traditional cultural and environmental skills to be used in the Heritage Centre as a pathway to higher skilled jobs and qualifications;
- employment that is based on manaakitanga and kaitiakitanga which restores cultural identity and pride; and
- Spin off traditional cultural opportunities through carving, weaving, waka tours, artists etc can all thrive and would enhance and compliment Te Hononga.

The Trust has commenced discussions with the government to work out how it might be able to take advantage of He Poutama Rangatahi - Youth Employment Pathways programme, to use the Centre as part of the pathway to employment for local rangatahi.



		<b>RS Eng Ltd</b> 09 438 3273 office@RSEng.co.nz 2 Seaview Road, Whangarei 0110
<b>The RUSSELL WHARF DEVELOPMENT</b> <b>VARIOUS WHARF UPGRADES - FAR NORTH</b>		
Client: <b>Far North Holdings Limited</b>		
Location: <b>THE STRAND RUSSELL</b>		
Date	25/01/18	Issued for information
Scale	1:500	Original A3
Drawn by	NW	Approved by
	Max	File 11753
		Sheet 2



**FAR NORTH HOLDINGS**  
**RUSSELL WHARF / KIOSK**  
**BAY OF ISLANDS**  
**FAR NORTH**  
**ENGINEERING SET**

OFFICIAL INFORMANT UNDER THE OFFICIAL INFORMATION ACT

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**GENERAL NOTES**

- All dimensions to be checked on site prior to commencement of work.
- Work only to figured dimensions, in the event of a discrepancy contact the Project Manager/ Designer
- Do not change any details without prior consent from the Project Manager
- Building Contractor to check all levels, dimensions, connections & Manufacturer's Specifications before beginning or manufacturing any work to ensure that all materials & labour necessary to complete the project has been allowed for, whether inferred, drawn on plans or not
- Liability will not be accepted by Designer for any materials or labour not shown on drawings or required by the council

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COVER SHEET & INDEX

RUSSELL WHARF / KIOSK  
 BAY OF ISLANDS, FAR NORTH

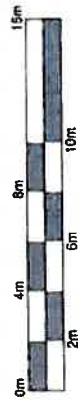
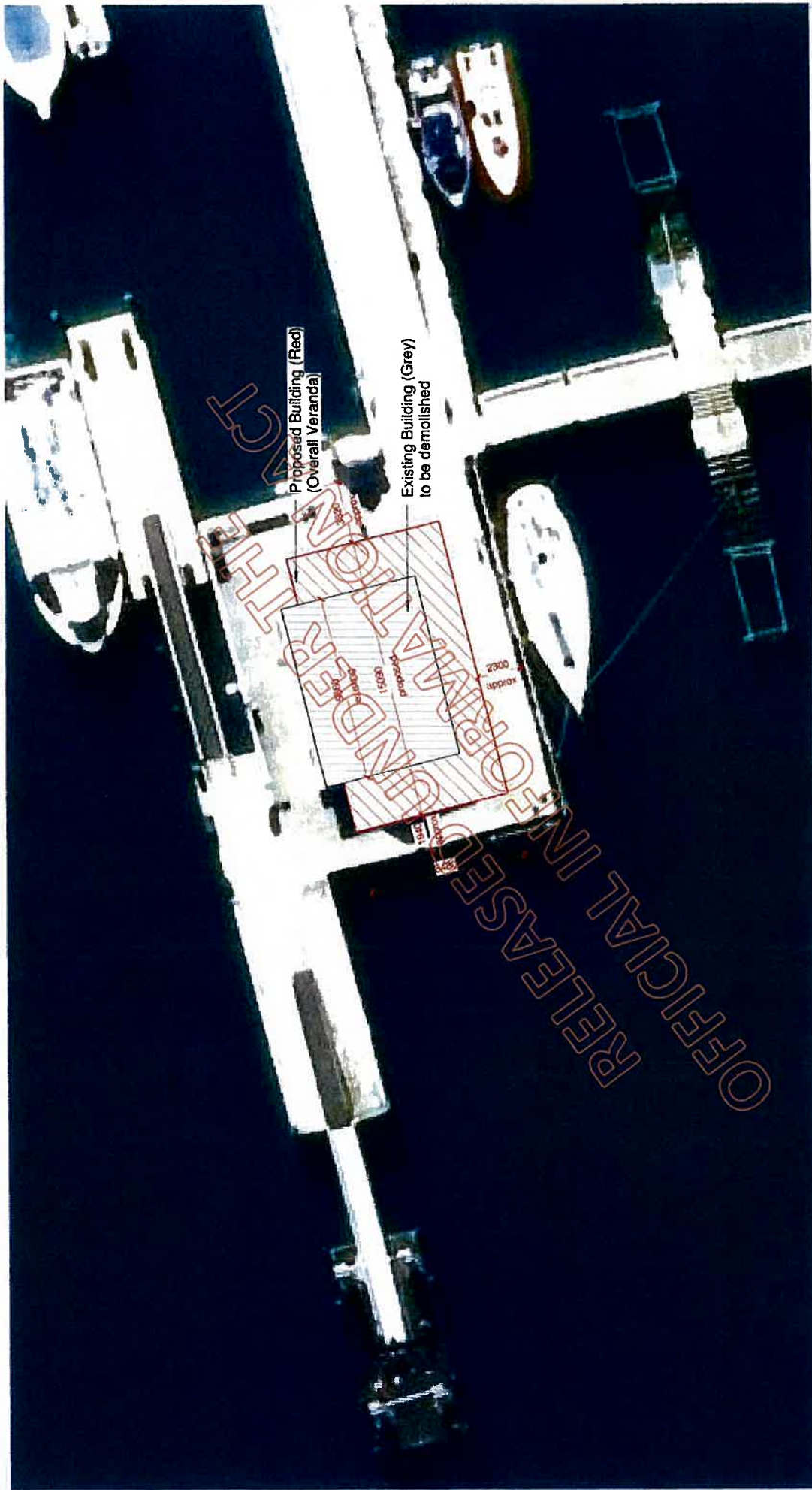
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**A-001**

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DATE PRINTED (19-10-17)

Sheet No

A-100

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SITE PLAN

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**FLOOR PLAN**

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Sheet No

**A-101**

**FLOOR PLAN NOTES:**

**General**

- All joinery sizes are to finish sizes add 7.5mm each side for box (unit) size
- Confirm kitchen layout with owner & kitchen manufacturer before commencing pipework
- All dimensions to timber framing not finished room sizes
- All framing & bottom plates to be H1 2 treated unless specified otherwise
- R2.4 polyester insulation to walls, R3.2 insulation to ceilings
- Smoke alarms to be installed to AS1570 & requirements. Equipment to comply with AS3786
- Joints between fixtures & wall linings; where basins, tubs or sinks abut impervious linings the joint between fixture & lining shall be sealed to prevent water penetration to concealed spaces or behind linings
- All ceilings to be lined with 13mm Gb standard except wet area ceilings are to be lined with 13mm Gib aqualine
- All walls to be 10mm standard gib except bractline panels & wet area walls & showers, wet area walls to be lined with 10mm gib aqualine and tiled
- Refer to bracing plan for bracing panel locations
- Tiling to bathrooms, toilets, entry to kitchen floors, 1/2 tile skirting to all WCs
- Showers to be tiled with recessed newline pro finish tray, sloping floor to central waste (level entry)
- Showers to have 6mm safety glass door unless specified
- All glazing to wet areas to be Grade A toughened safety glass
- All access routes, both external and internal, provide anti-slip surfaces complying with NZBC clause D1/AS1 (2.1 Slip Resistance)
- Windows W01-03 & W08-10 are located above car plane and are located on North & South Elevations & Window Schedule or these plans

**LEGEND**

- Raking Ceiling Area
- Smoke alarms to be installed to AS1570 & requirements. Equipment to comply with AS3786

Area  
73.2m<sup>2</sup>





**FLOOR PLAN NOTES:**

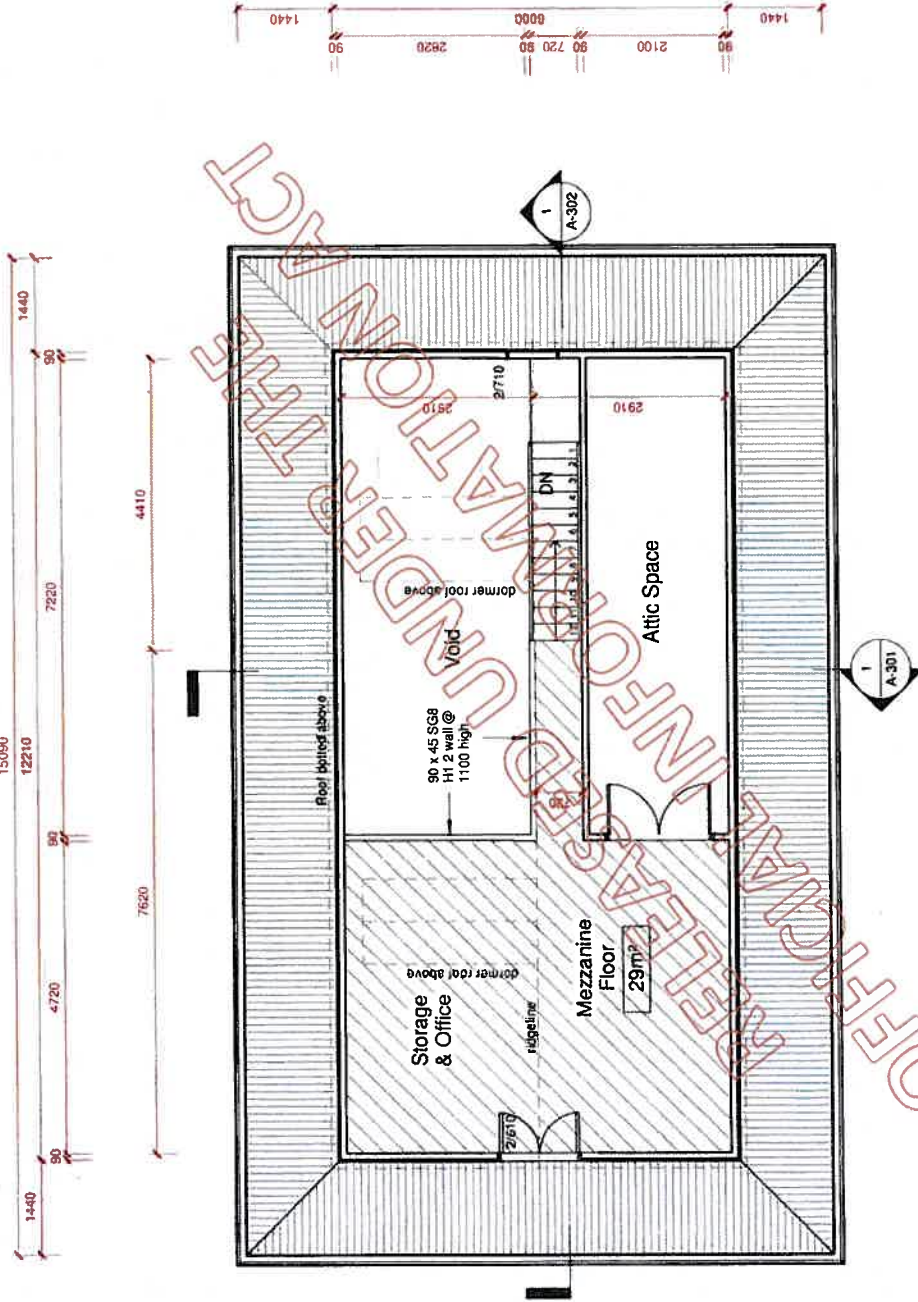
**General**

- All joinery sizes are to finish sizes add 7.5mm each side for box (unit) size
- Confirm kitchen layout with owner & kitchen manufacturer before commencing pipework
- All dimensions to timber framing not finished room sizes
- All framing & bottom plates to be H1.2 treated unless specified otherwise
- R2.4 polyster insulation to walls,
- R3.2 polyster insulation to ceilings
- Smoke alarms to be installed to AS1670.6 requirements. Equipment to comply with AS3786
- Joins between fixtures & wall linings; where basins, tubs or sinks about impervious linings the joint between fixture & lining shall be sealed to prevent water penetration to concealed spaces or behind linings
- All ceilings to be lined with 13mm Gb standard except wet area callings are to be lined with 13mm Gb aqualine
- All walls to be 10mm standard gib except bracceline panels & wet area walls & showers, wet area walls to be lined with 10mm gib aqualine and tiled
- Refer to bracing plan for bracing panel locations
- Tiling to bathrooms, toilets, entry laundry & kitchen floors, 1/2 tile skirting to all WC's
- Showers to be tiled with recessed newline pro finish tray, sloping floor to central waste (level entry)
- Showers to have 6mm safety glass door unless specified
- All glazing to wet areas to be Grade A to glass to safety glass
- All access routes, both external and internal, provide anti-slip surfaces complying with NZBC clause D1/AS1 (2.1 Slip Resistance)
- Windows W01-03 & W08-10 are located above cut plane and are located on North & South Elevations & Window Schedule of these plans

**LEGEND**

Raking Ceiling Area

Smoke alarms to be installed to AS1670.6 requirements. Equipment to comply with AS3786



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A-102

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BAY OF ISLANDS, FAR NORTH

FIRST FLOOR PLAN

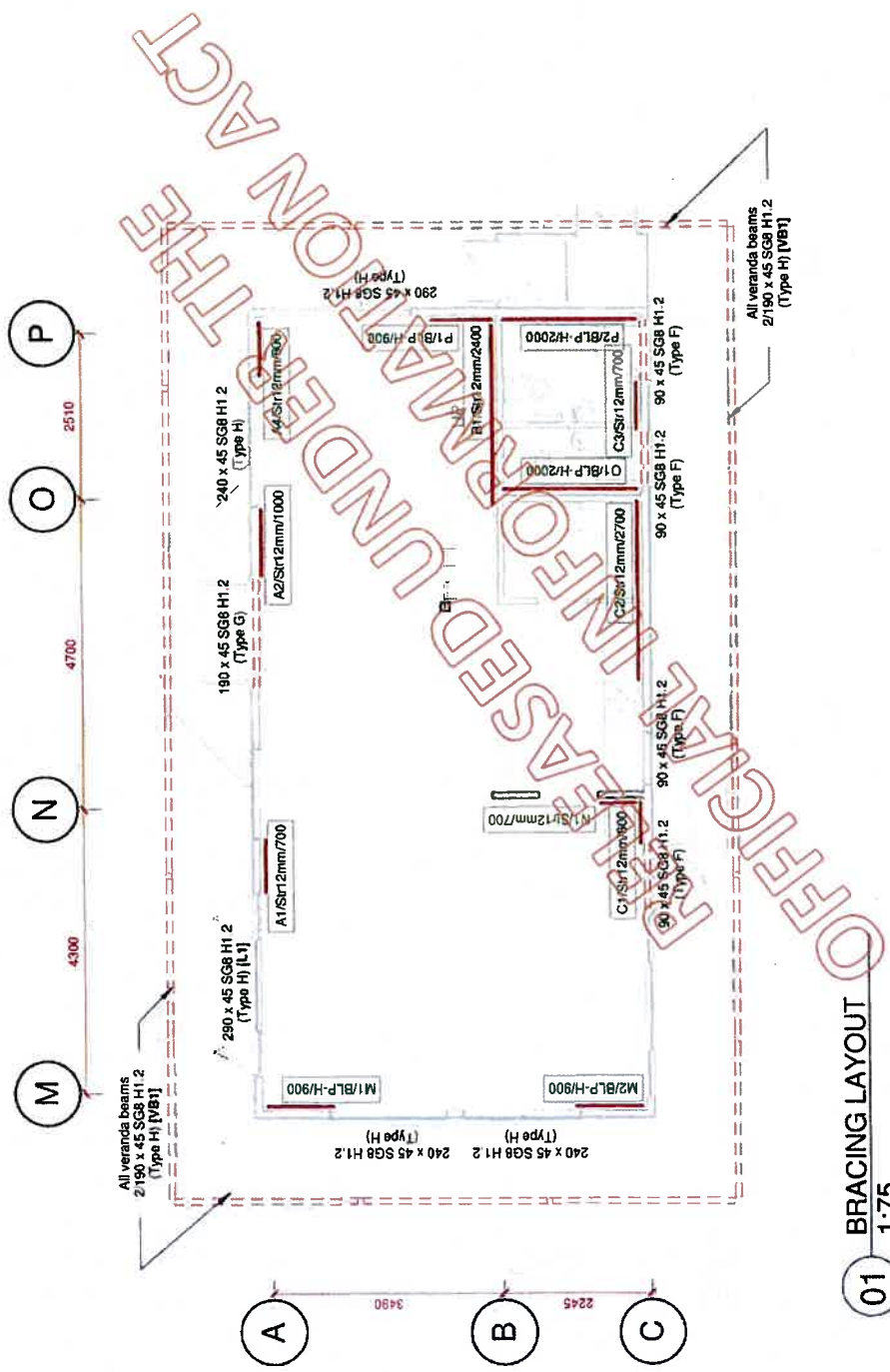
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BRACING KEY		
Element Number	Element Length	Bracing Type
P1/Sr12mm/900	→	→

**NOTE:**  
External walls to be constructed off site as 8 panels, fully clad on exterior & craned into position



Area  
73.2msq

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BRACING LAYOUT

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**NOTES:**

**Access Routes**  
To all access routes both external and internal, provide anti-slip surface complying NZ EC D/IAS1

Footing depth into good ground as defined in NZS3604:2011

Concrete footing strength to 25MPa

All new subfloor deck fixings to be Stainless Steel

**Legend**



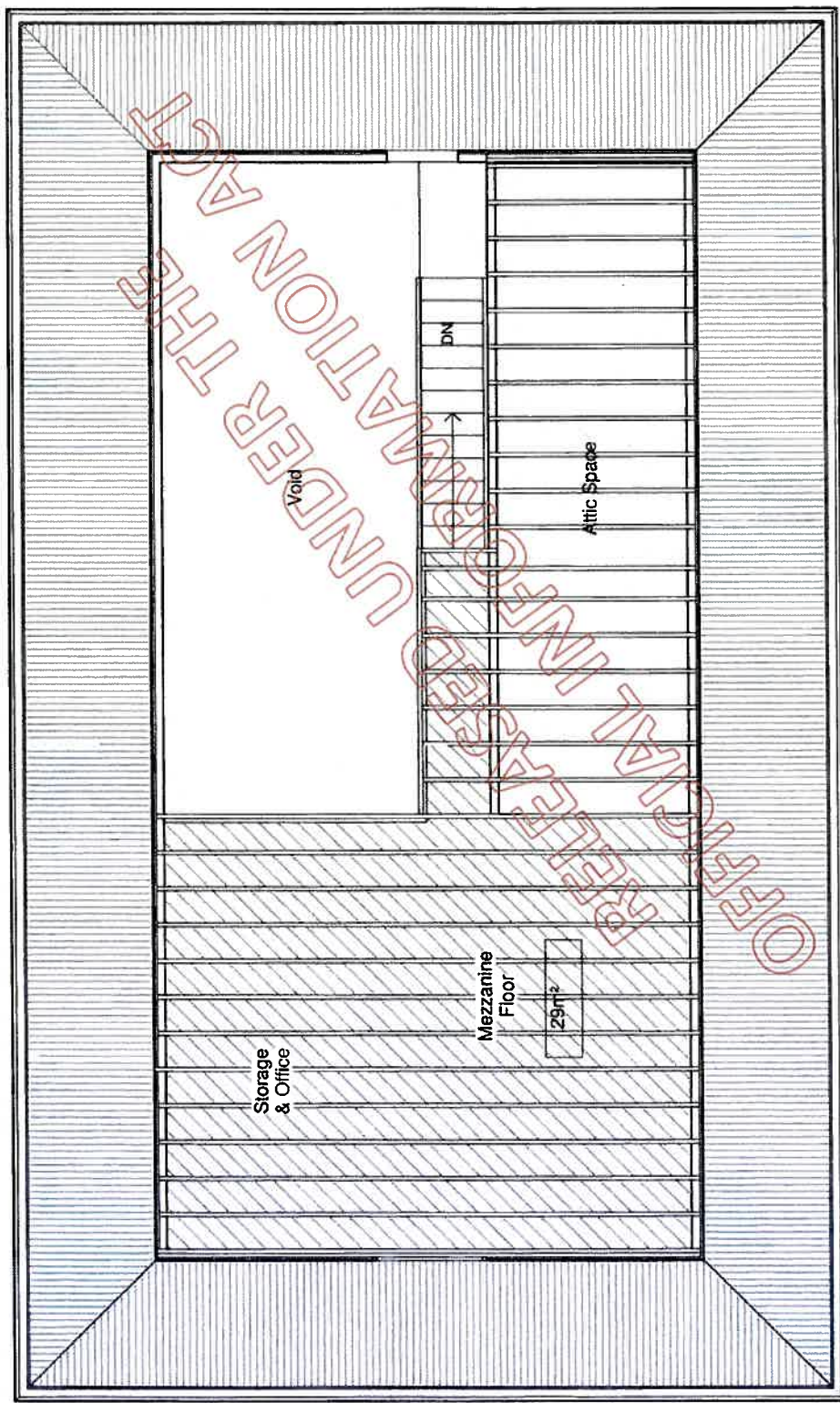
**Beaters**  
2/190 x 45 SGB H3 2 with SS 2/Sheathed nails to joists.



**Joists**  
[House] -190 x 45 SGB H1 2 @ 400cra max  
[Deck] -190 x 45 SGB H3 2 @ 400cra max with SS joist hangers to stringers



**Stringers**  
190 x 45 SGB H3 2 with SS Trubolts and 50sq washers @ 900cra max



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**A-104**

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**MEZZANINE SUBFLOOR LAYOUT**

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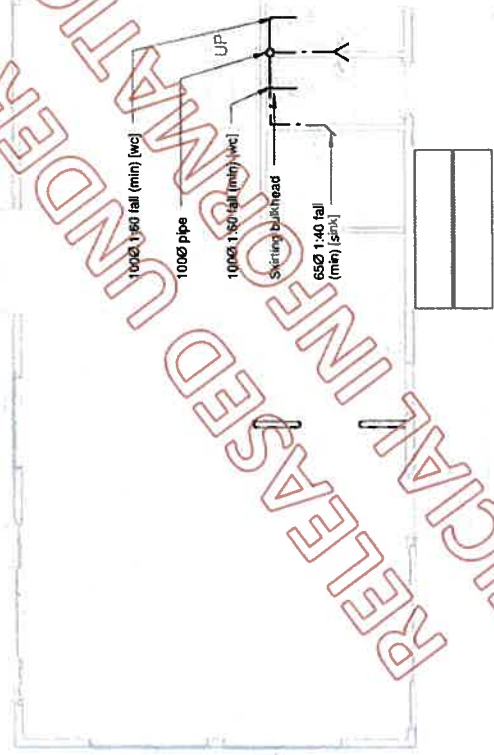




Area  
73.21m<sup>2</sup>

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**PLUMBING NOTES:**

All plumbing and drainage to comply with AS/NZS 3500 'The National Plumbing and Drainage Code'

Install all plumbing 100mm below concrete floor

Check position of sewer & stormwater laterals entering site before start of work

**LEGEND**

- AAV - Air Admittance Valve
- ORGT - Overflow Relief Gully Trap
- TV - Terminal Vent
- IB - Inspection Bend

**WASTE PIPE GRADIENTS**

- 400 1:40 fall (min)
- 650 1:40 fall (min)
- 1000 1:80 fall (min)



**ROOF PLAN NOTES:**

**ROOF**

0.40s Corrugated Colorsteel Maxx roofing with 0.50 soft edge flashings to match @ 5° on galvanised wire mesh roofing underlay with 8g blue screws @ 800cbs max. on 70 x 45 SG8 H1.2 purlins on flat @ 900cbs max.

**FASCIA & SPOUTING**

200 x 40mm H3.1 preprimed fascia with 125mm Colorsteel Box Gutters

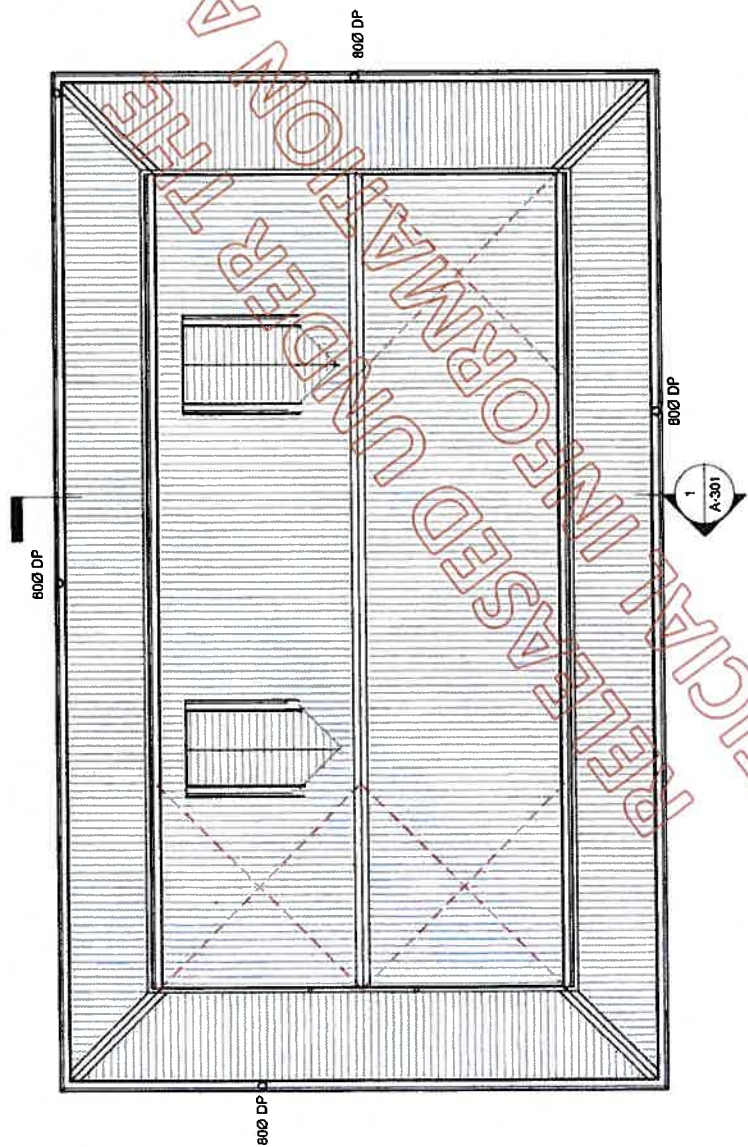
**DOWNPIPE**

800 u.P.V.C downpipes paint finish

Line of soffit

**ROOF PLANE BRACING**

diagonally opposing pair tensioned lumberlok strip brace roof plane bracing running from ridge to top plate installed as per AS/NZS 4600:3 & 1200:3 15 nails each end & 1200:3 15 nail at crossing (after tensioning)



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A-107

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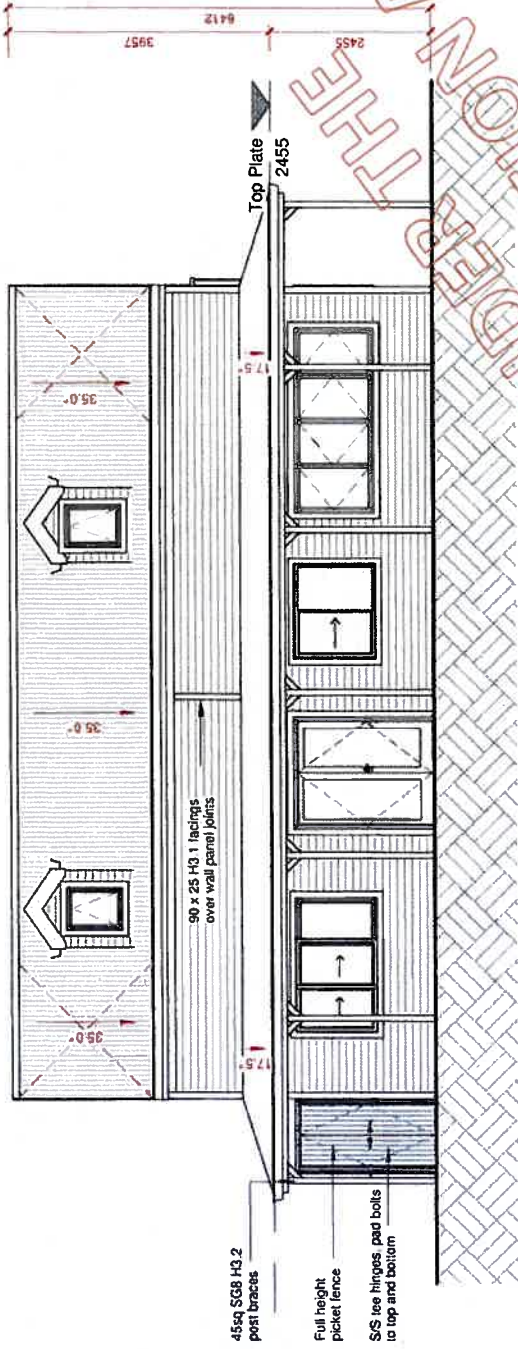
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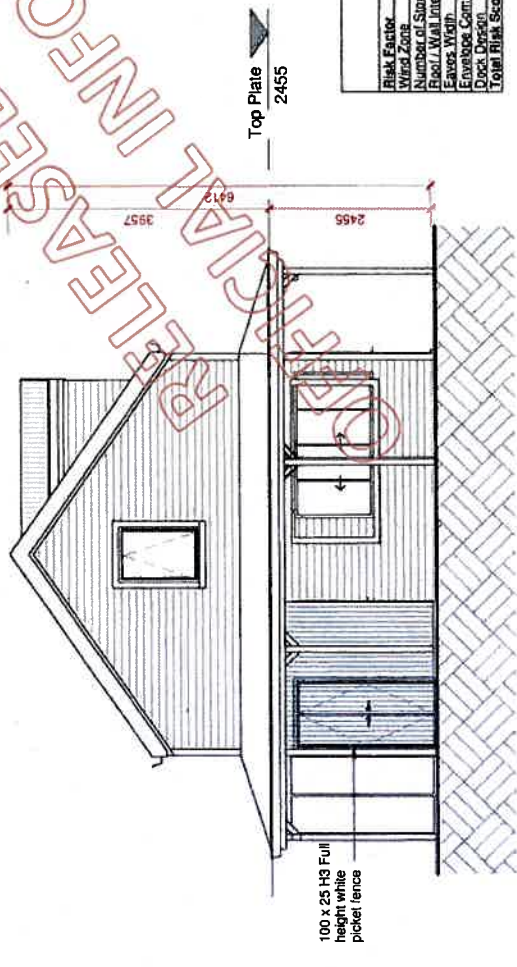
ROOFING LAYOUT

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01 NORTH ELEVATION  
1:75



02 EAST ELEVATION  
1:75

### NORTH ELEVATION RISK MATRIX

Risk Factor	Risk Severity	Risk Score
Wind Zone	Very High	2
Number of Storeys	High	2
Roof / Wall Intersection Design	Medium	1
Eaves Width	Low	0
Envelope Complexity	Low	0
Deck Design	Low	0
<b>Total Risk Score</b>		<b>5</b>

**ELEVATION NOTES**

- ROOFING**  
0.4sq Corrugated ColorCote AlumiGuard ARX Roofing with 0.5q soft edge fastenings to match, @ 35' & 17.5' on self supporting underlay with 6g blue screws @ 600mm max
- FASCIA & SPOLTING**  
200 x 40 H3 1 extruded fascia & 125mm Colorsteel Box Gutters
- DOWNPIPES**  
80a u P V C downpipes  
Install to manufacturers recommendations
- SOFFIT LINING**  
JH 4.5mm hardiflex soffit lining with u P V C joiners  
Install to manufacturers recommendations
- WALLS** Linea Weatherboard  
JH Linea Weatherboard 150mm lapped on 20mm drained cavity battens, over fastwrap building paper
- JOINERY**  
Powdercoated aluminium joinery  
Cover the glazed walls with support bars  
H3 1 timber rains  
Install to manufacturers recommendations  
H3 1 timber facings on all windows and doors

### H1 CALCULATIONS - SCHEDULE (GREAT NORTHERN METHOD)

Element	Required R-Value	Achieved R-Value
Wall Insulation	R1.9	R2.1
Floor Insulation	R2.9	R3.1
Floor Insulation	R1.3	R1.6
Glazing	R0.26	R0.26

Note: Construction R-Values are sourced from BRANZ House Insulation Guide or Design Navigator

### GLAZING VS WALL CALCULATIONS

Wall Facings	m <sup>2</sup> of wall	m <sup>2</sup> of windows	Window % of wall
North	19.26	8.26	42.73%
East	4.4	4.27	97%
South	29.3	9.45	32.23%
West	129.08	41.6	32.38%
<b>Total</b>			

Total glazing of walls is less than 50% of total wall area so passes the H1 Great Northern Glazing Method

### EAST ELEVATION RISK MATRIX

Risk Factor	Risk Severity	Risk Score
Wind Zone	Very High	2
Number of Storeys	High	2
Roof / Wall Intersection Design	Low	0
Eaves Width	Low	0
Envelope Complexity	Low	0
Deck Design	Low	0
<b>Total Risk Score</b>		<b>4</b>

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**EAST ELEVATION**  
1:75

ELEVATIONS (NORTH & EAST)

RUSSELL WHARF / KIOSK  
BAY OF ISLANDS, FAR NORTH

REVISIONS

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**SOUTH ELEVATION RISK MATRIX**

Risk Factor	Risk Severity	Risk Score
Wind Zone	Very High	2
Number of Storeys	1	1
Roof / Wall Intersection Detail	Low Risk	0
Eaves Width	Medium Risk	1
Envelope Compatibility	Low Risk	0
Deck Design	Low Risk	0
<b>Total Risk Score</b>		<b>4</b>

**ELEVATION NOTES**

**ROOFING**  
 449 Coloured ColorCoat Alumicard ARX  
 Panels with 50mm soft edge fascias to  
 match @ 35° & 17.5° eaves. All soffits  
 underlay with 8g blue screws @ 600c/s max

**FASCIA & SCOUTING**  
 200 x 40 H3.1 preformed fascia &  
 125mm Colorsteel Box Gutters

**DOWNPIPES**  
 80e u P V C downpipes  
 install to manufacturers recommendations

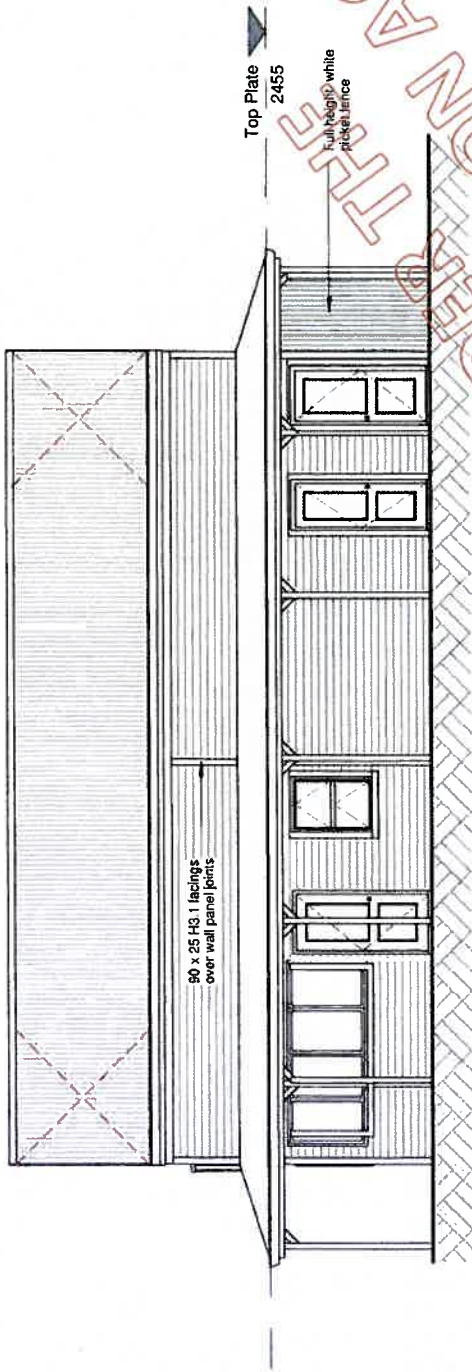
**SOFFIT LINING**  
 JH 4 5mm hardiflex soffit lining  
 with u P V C joints  
 install to manufacturers recommendations

**WALLS** Linea Weatherboard  
 JH Linea Weatherboard 150mm lapped on  
 20mm drained cavity battens, over fastwrap  
 building paper

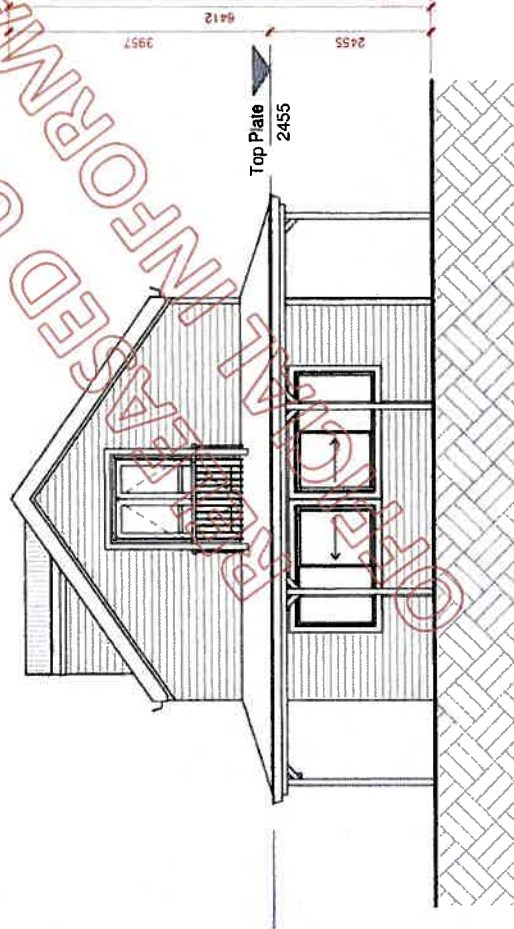
**JOINERY**  
 Powdercoated aluminium joinery  
 double glazed with sill support bars  
 H3.1 timber reveals  
 install to manufacturers recommendations  
 H3.1 timber fascias on all windows and doors

**WEST ELEVATION RISK MATRIX**

Risk Factor	Risk Severity	Risk Score
Wind Zone	Very High	2
Number of Storeys	1	1
Roof / Wall Intersection Detail	Low Risk	0
Eaves Width	Medium Risk	1
Envelope Compatibility	Low Risk	0
Deck Design	Low Risk	0
<b>Total Risk Score</b>		<b>4</b>



**01 SOUTH ELEVATION**  
 1:75



**02 WEST ELEVATION**  
 1:75

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Sheet No. A-202

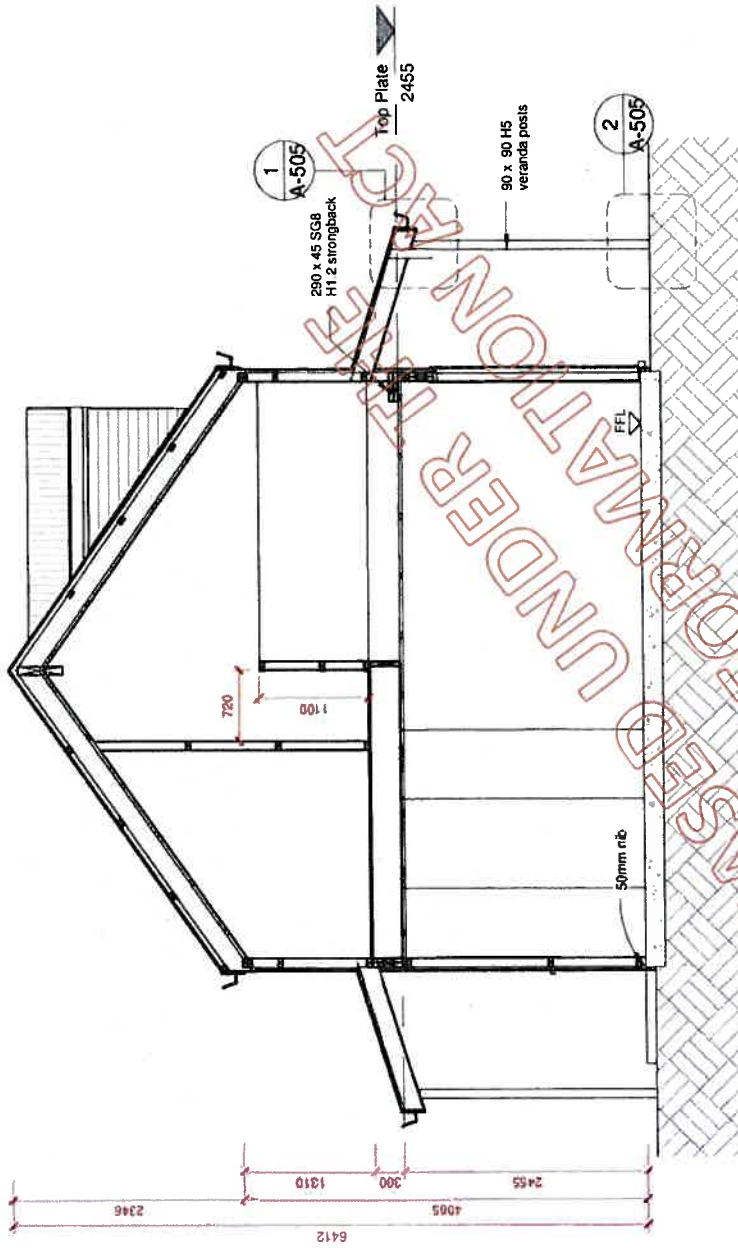
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RUSSELL WHARF / KIOSK  
 BAY OF ISLANDS, FAR NORTH

ELEVATIONS (SOUTH &  
 WEST)

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01 SECTION A-A  
1:50

**ROOFING**  
0.4kg Comrigated ColorCote Alum/Gard ARX Roofing with 0.5g soft edge flashings to pitch @ 35° & 17.5° on self supporting walls with 80 Blue screws @ 600cirs max. on 140 x 45 SGB H1.2 panels on flat @ 300cirs max. Fastwrap building paper laid horizontally

**FASCIA SPOUTING & DOWNPIPES**  
200 x 40 H3.1 preprimed fascia & 125mm Colorsteel Box Gutter with 800 u.p.v.c downpipes  
Install to manufacturers recommendations

**SOFFIT LINING**  
JH 7.5mm HardiGroove soffit lining with u.p.v.c. joints  
Install to manufacturers recommendations

**RAFTERS**  
180 x 45 SGB H1.2 @ 600cirs with CT200 fixings @ each end

**EXTERIOR WALLS & INSULATION**  
50x45 H1.2 SGB external wall framing studs @ 400cirs, nogs @ 800cirs @ 100.0, 2.4m  
290 45 H1.2 SGB external wall framing studs @ 600cirs, nogs @ 800cirs @ 100.0, 3.0m  
140 x 45 H1.2 SGB external wall framing studs @ 400cirs, nogs @ 800cirs @ 100.0, 1.5m  
2140 x 45 H1.2 SGB external wall framing studs @ 400cirs, nogs @ 800cirs @ 100.0, 2.2m  
R2.4 insulation to external walls  
Install to manufacturers recommendations

**WALLS VERTICAL SHIPLAP**  
JH Linea Weatherboard with 1.55mm cover Direct Fixed JH Linea Weatherboard with 155mm cover over 20mm drained cavity

**RIGID AIR BARRIER**  
7mm H3.2 Ecoply Barrier Panel

**INTERIOR WALLS**  
90x45 H1.2 SGB internal wall framing studs @ 400cirs, nogs @ 800cirs @ 100.0, 2.4m  
290 45 H1.2 SGB internal wall framing studs @ 600cirs, nogs @ 800cirs @ 100.0, 3.0m  
10mm g/b standard to each side (aqualine to wet areas)

**JOINERY**  
Powder coated aluminium joinery double glazed with eil support bars powder coated head flashings to match H3.1 timber reveals to suit anchorages  
Install to manufacturers recommendations

**CEILING**  
125mm g/b standard ceiling lining screw fixed to u/s of rondo ceiling battens clip fixed (acoustic clips) @ 600cirs (aqualine to wet areas)  
R3.2 blanket insulation to all ceilings

**TOP PLATE**  
290 x 45 SGB H1.2 with 2/End Nails & SNS9L fixings to studs

**BOTTOM PLATE**  
90 x 45 SGB H1.2 on DPC with M12 Trubolts & 50sq washers @ 900cirs

**POWDER COATED ALUMINIUM JOINERY**  
Double glazed with eil support bars powder coated head flashings to match H3.1 timber reveals to suit anchorages  
Install to manufacturers recommendations

**CONCRETE SLAB**  
25MPa concrete floor slab 100mm reinforced with SE62

0.25mm polythene VPM on 50mm thick sand bedding layer extending 100mm thick min. over perimeter compact in 150mm layers

**FLOORING**  
20mm Sitonboard flooring

**DECKING**  
90 x 35 H3.2 premium radiata pine decking planks with 2mm gap, 12mm gap between decking and cladding

**BASE CLADDING**  
7.5mm JH HardiFlex base cladding with S/S fixings

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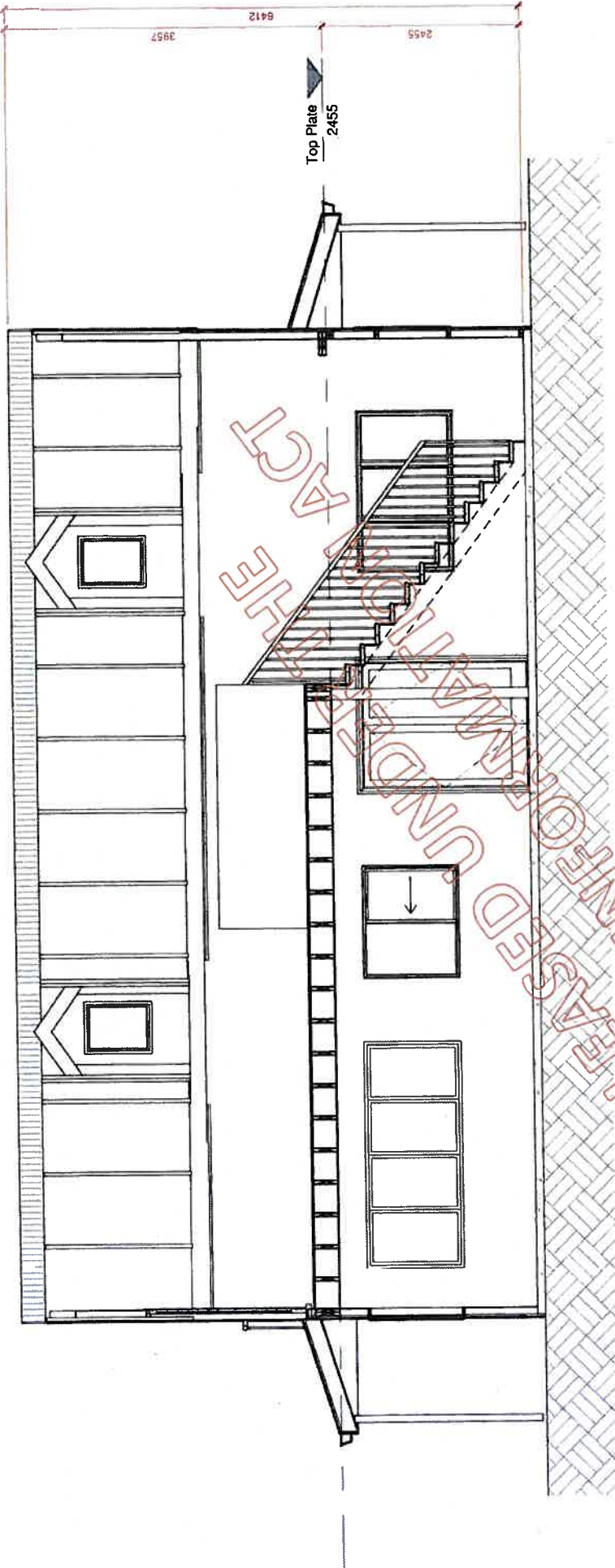
RUSSELL WHARF / KIOSK  
BAY OF ISLANDS, FAR NORTH

SECTION A-A

40 Mathews Ave 0410 Victoria  
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**archline**  
architecture

Double Rafter to each sides of dormers



01 SECTION B-B  
1:7.5

**ROOFING**  
0.40g Convagated ColorCote AlumGard ARX Roofing with 0.5g soft edge flashings to match, 35° & 17.5° on self supporting underlay with 8g blue screws @ 600c/s max. on 70 x 45 SG8 H1.2 purlins on flat @ 800c/s max., Fastwrap building paper laid horizontally

**FASCIA SPOULTING & DOWNPIPES**  
200 x 40 H3.1 preprimed fascia & 125mm Colorsteel Box Gutter with 800 u P V C downpipes  
Install to manufacturers recommendations

**SOFFIT LINING**  
JH 7.5mm HardGroove soffit lining with u.P.V.C. panans  
Install to manufacturers recommendations

**RAFTERS**  
190 x 45 SG8 H1.2 @ 600c/s with C1200 linings @ each end

**EXTERIOR WALLS & INSULATION**  
90x45 H1.2 SG8 external wall framing studs @ 600c/s, nogs @ 800c/s (up to 2.4m)  
290 45 H1.2 SG8 external wall framing studs @ 600c/s, nogs @ 800c/s (up to 2.0m)  
140 x 45 H1.2 SG8 internal wall framing studs @ 400c/s, nogs @ 800c/s (up to 2.0m)  
2140 x 45 H1.2 SG8 external wall framing studs @ 400c/s, nogs @ 800c/s (up to 2.0m)  
R2.4 insulation to exterior walls  
Install to manufacturers recommendations

**WALLS VERTICAL SHIPLAP**  
JH Linea Weatherboard with 155mm cover Direct Fixed  
JH Linea Weatherboard with 155mm cover over 20mm drained cavity

**RIGID AIR BARRIER**  
7mm H3.2 Ecoply Barrier Panel

**INTERIOR WALLS**  
90x45 H1.2 SG8 internal wall framing studs @ 400c/s, nogs @ 800c/s (up to 2.4m)  
290 45 H1.2 SG8 internal wall framing studs @ 600c/s, nogs @ 800c/s (up to 2.0m)  
10mm gib standard to each side (equalline to wet areas)

**TOP PLATE**  
290 X 45 SG8 H1.2 with 2/End Nails & SNSOL fixings to studs

**BOTTOM PLATE**  
90 x 45 SG8 H1.2 on DPC with M12 Trubolts & 50sq washers @ 900c/s

**JOINERY**  
Powder coated aluminium joinery double glazed with all support bars powder coated head flashings to match H3.1 timber reveals to suit architraves  
Install to manufacturers recommendations

**CEILING**  
13mm gip standard ceiling lining screw fixed to us of rondo ceiling battens clip fixed (acoustic clips) @ 600c/s (equalline to wet areas)  
R3.2 blanket insulation to all ceilings

**CONCRETE SLAB**  
25MPa concrete floor slab 100mm reinforced with SE84 grade 500E mesh with 0.25mm polyurethane DPA on 20mm thick building paper extending 100mm thick min. compacted hardfill extending 1m east building perimeter compact in 150mm layers

**FLOORING**  
20mm Strandboard flooring

**DECKING**  
90 x 35 H3.2 premium radiata pine decking planks with 2mm gap, 12mm gap between decking and cladding

**BASE CLADDING**  
7.5mm JH HarderFlex base cladding with S/S fixings

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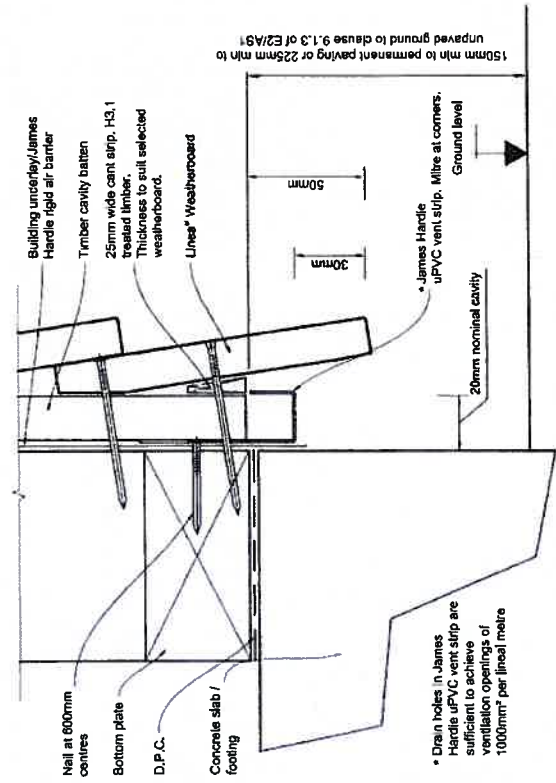
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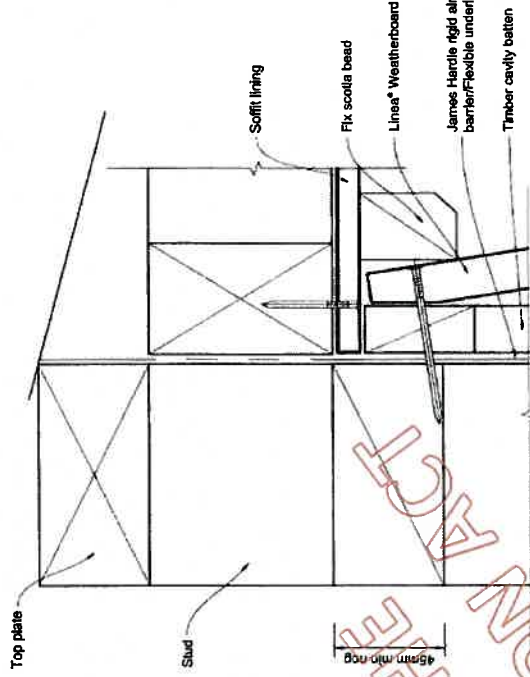
RUSSELL WHARF / KIOSK  
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SECTION B-B

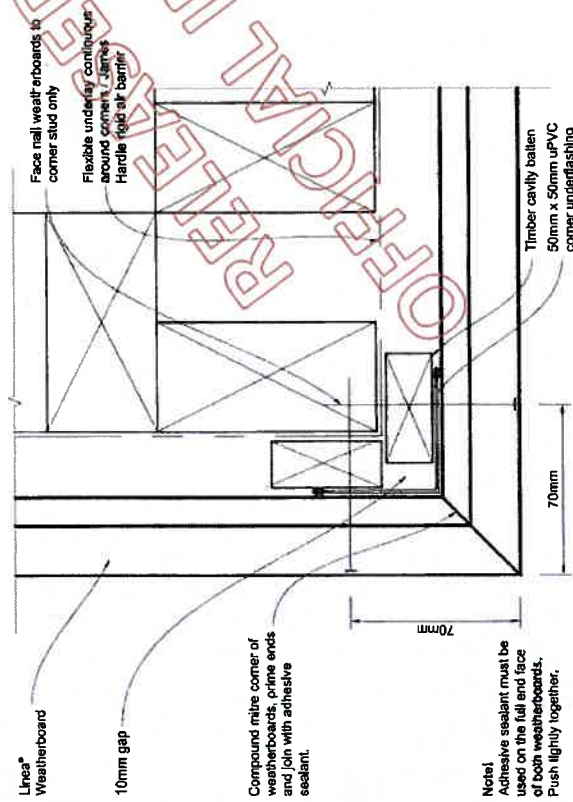


01 LINEA WEATHERBOARD (BOTTOM PLATE)  
NTS

\* Drain holes in James Handle uPVC vent strip are sufficient to achieve ventilation openings of 1000mm<sup>2</sup> per lineal metre

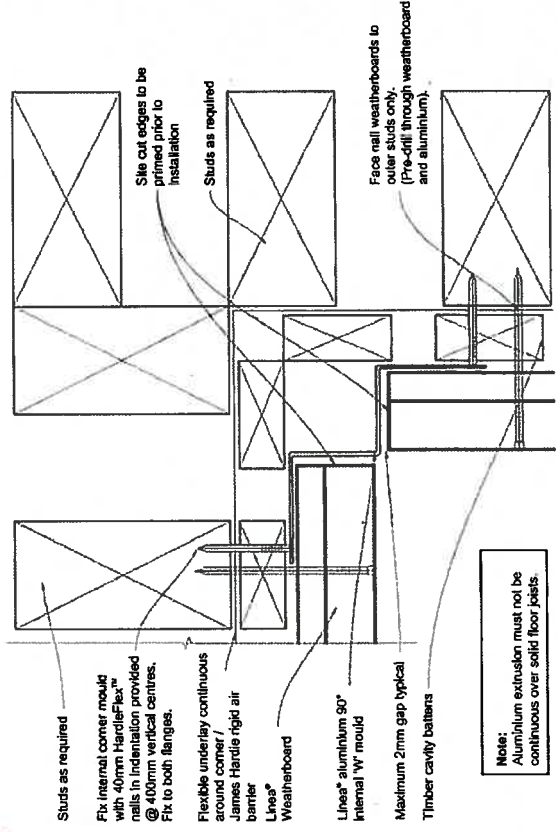


02 LINEA WEATHERBOARD (SOFFIT)  
NTS



03 LINEA WEATHERBOARD (EXTERNAL CORNER)  
NTS

Note!  
Adhesive sealant must be used on the full end face of both weatherboards. Push tightly together.



04 LINEA WEATHERBOARD (INTERNAL CORNER)  
NTS

Note:  
Aluminium extrusion must not be continuous over solid floor joists.

WALL DETAILS

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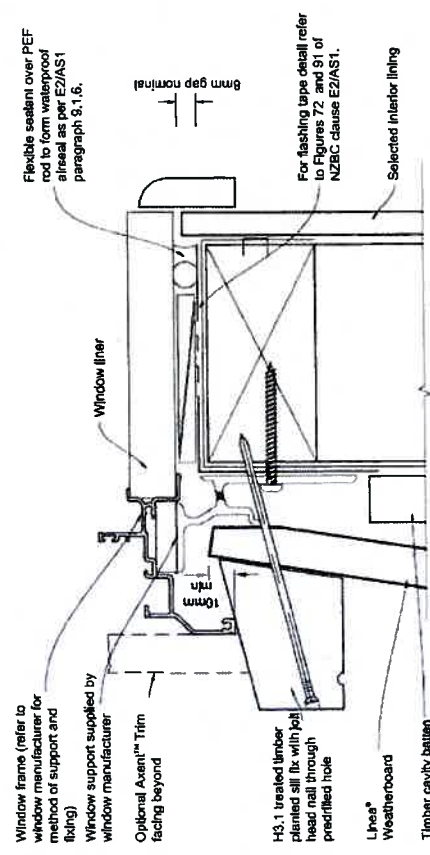
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Sheet No

A-501

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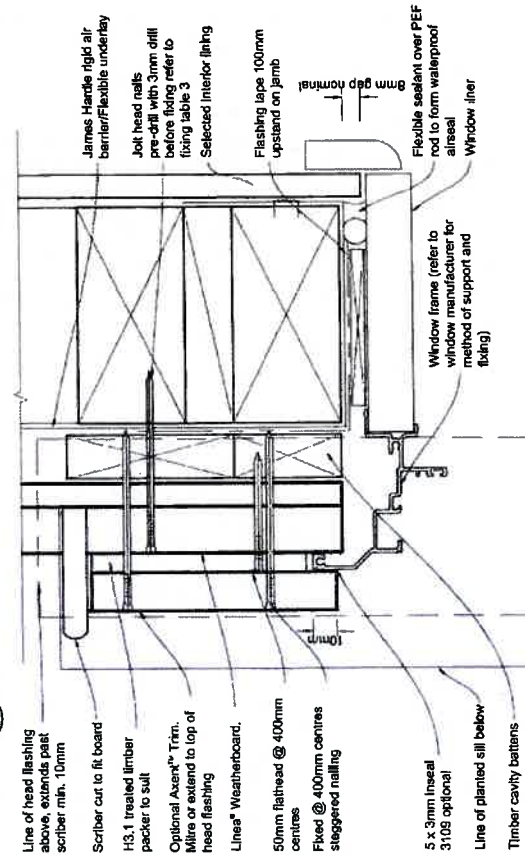


**General notes for materials section**

1. Flashing materials must be selected based on environmental exposure, refer to NZS 3604 and Table 20 of NZBC clause E2/AS1.
2. Flexible underlay must comply with acceptable solution E2/AS1.
3. Flashing tape must have proven compatibility with the selected flexible underlay / James Hardie rigid air barrier and other materials with which it comes into contact.

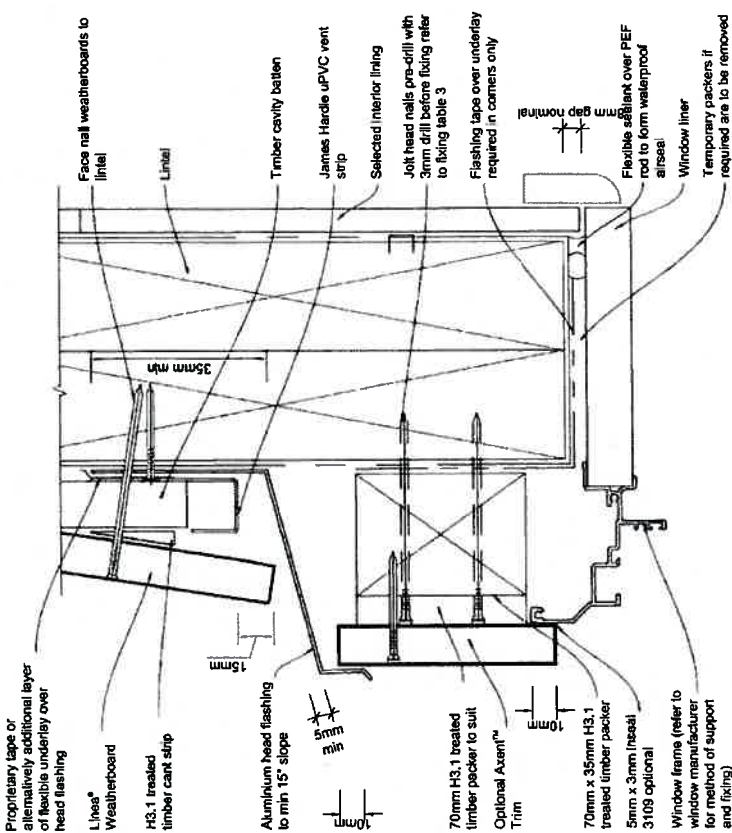
Refer to the manufacturer or supplier for technical information for these materials.

**01 LINEA WEATHERBOARD (SILL)**  
NTS



Note:  
When James Hardie rigid air barrier is used flashing tape to be applied to the entire window opening.

**03 LINEA WEATHERBOARD (JAMB)**  
NTS



Note:  
When James Hardie rigid air barrier is used flashing tape to be applied to the entire window opening.  
Sealant must be installed between head flashing and trim in VJ and E/W joint zones and SED pressures.  
Alternatively, the head flashings can be formed with stop ends as per E2/AS1

**02 LINEA WEATHERBOARD (HEAD)**  
NTS

WALL DETAILS

RUSSELL WHARF / KIOSK  
BAY OF ISLANDS, FAR NORTH

Scale: 1 : 5  
Drawn By: Designer

Sheet No

A-502

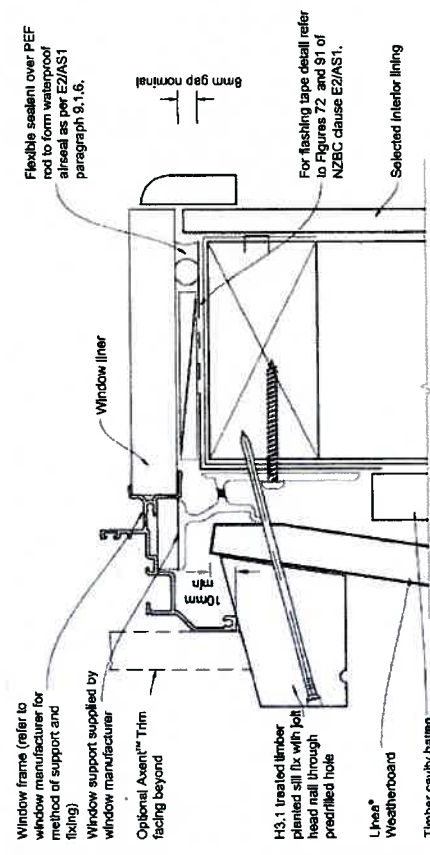
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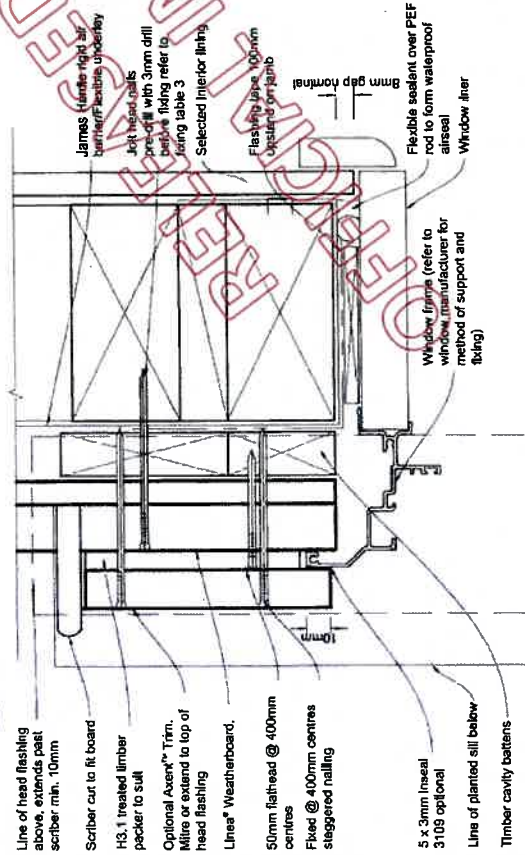


**General notes for materials section**

1. Flashing materials must be selected based on environmental exposure, refer to NZS 3604 and Table 20 of NZBC clause EZIAS1.
2. Flexible underlay must comply with acceptable solution EZIAS1.
3. Flashing tape must have proven compatibility with the selected flexible underlay / James Hardie rigid air barrier and other materials with which it comes into contact.

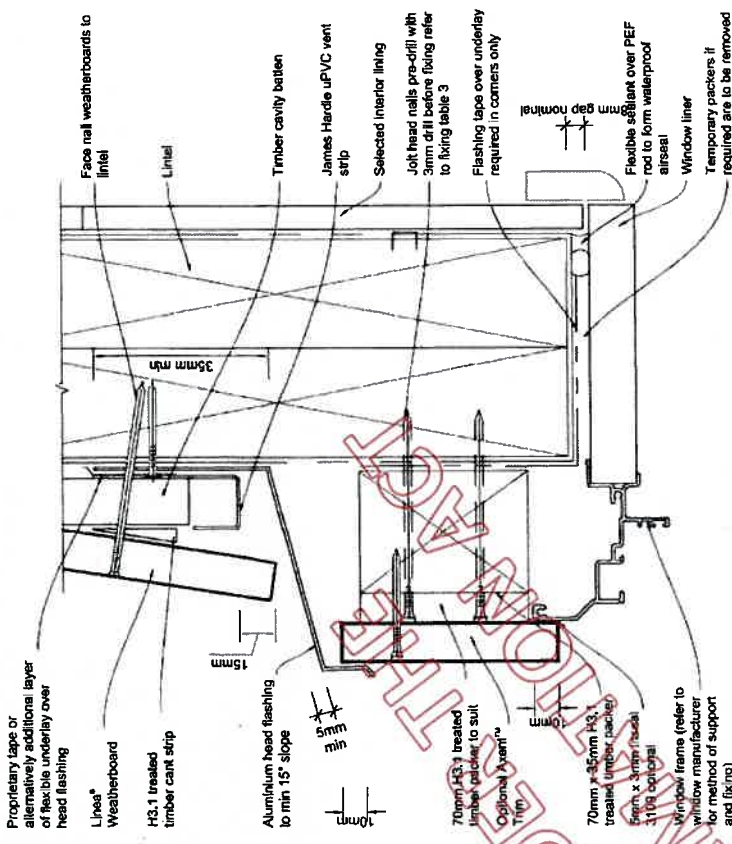
Refer to the manufacturer or supplier for technical information for these materials.

**01 LINEA WEATHERBOARD (SILL)**  
NTS



Note:  
When James Hardie rigid air barrier is used flashing tape to be applied to the entire window opening.

**03 LINEA WEATHERBOARD (JAMB)**  
NTS



Note:  
When James Hardie rigid air barrier is used flashing tape to be applied to the entire window opening.

- Sealant must be installed between head flashing and trim in VH and EH wind zones and SED pressures.
- Alternatively, the head flashings can be formed with stop ends as per EZIAS1

**02 LINEA WEATHERBOARD (HEAD)**  
NTS

WALL DETAILS

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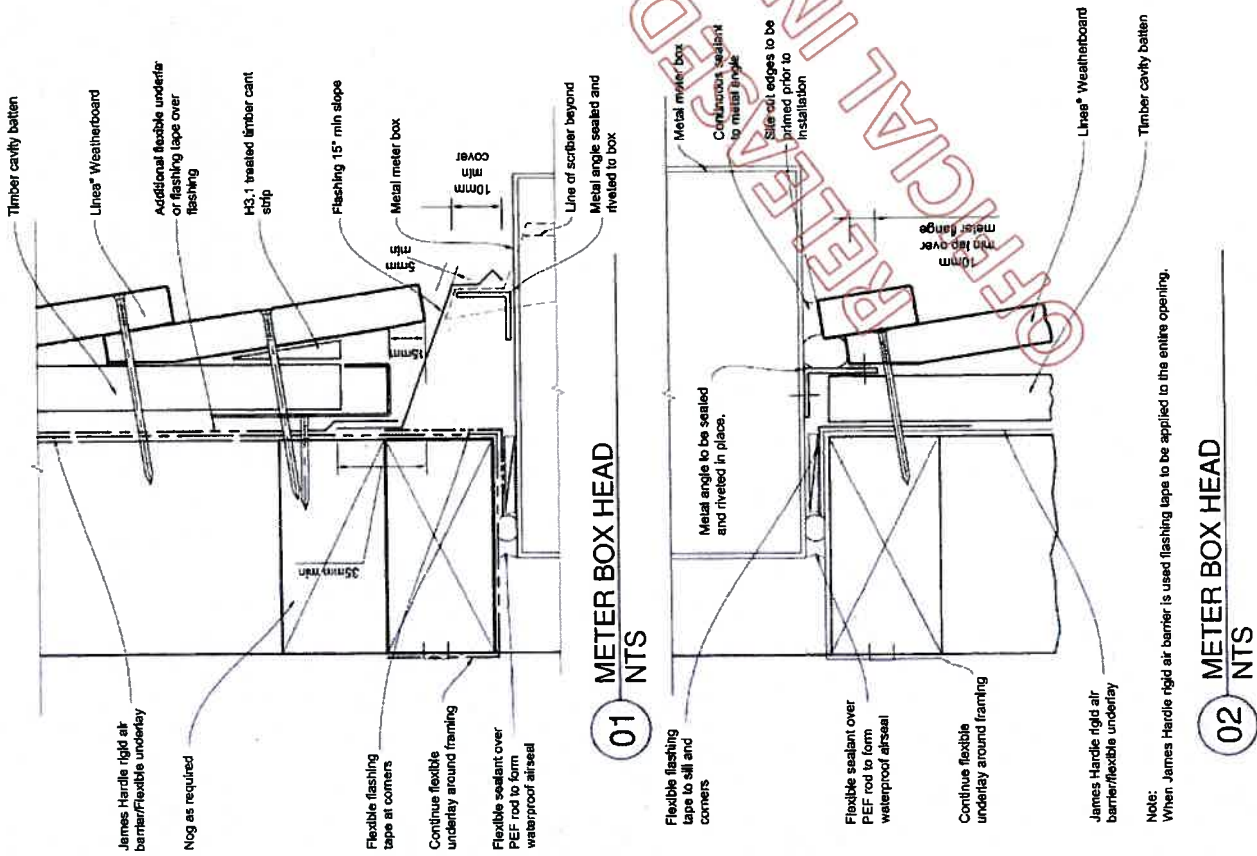
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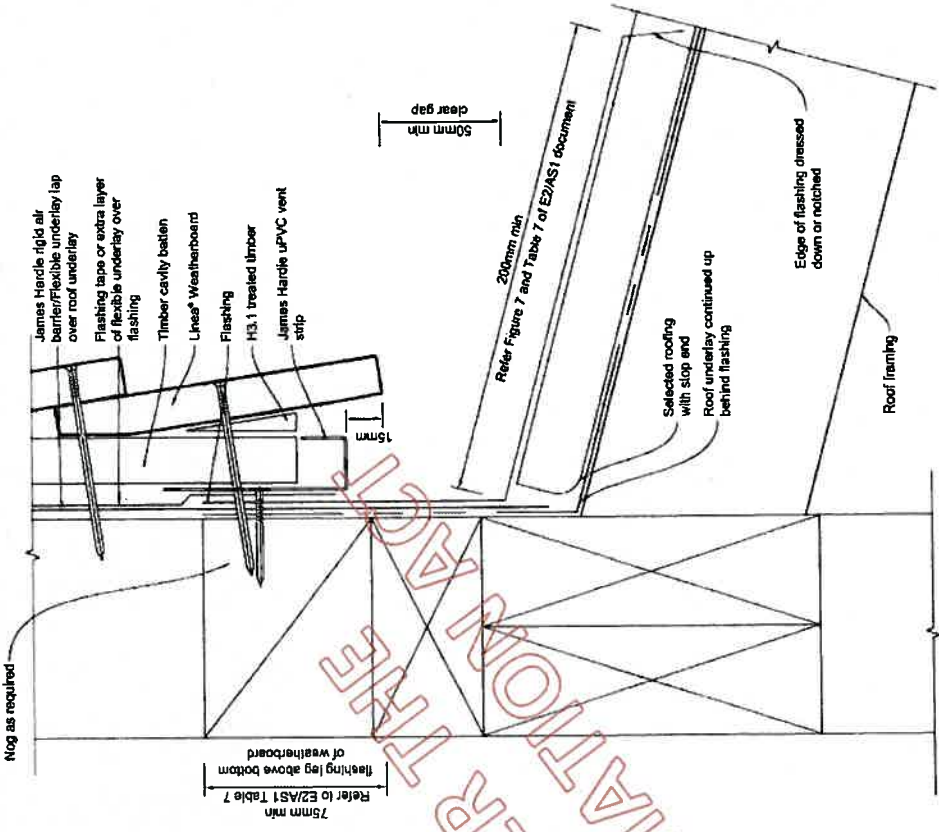
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A-502



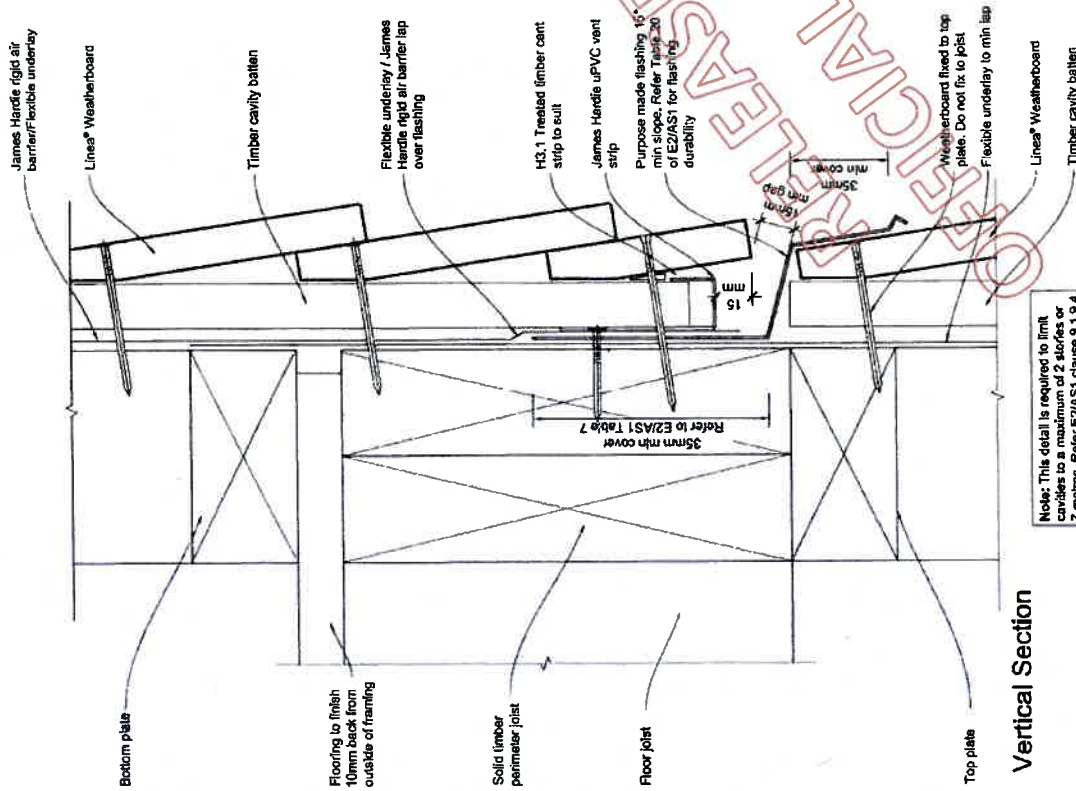
01 METER BOX HEAD  
NTS



03 LINEA WEATHERBOARD (APRON FLASHING)  
NTS

02 METER BOX HEAD  
NTS

Note: When James Hardie rigid air barrier is used flashing tape to be applied to the entire opening.



Vertical Section

01 LINEA WEATHERBOARD HORIZONTAL JOINT

NTS

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WALL DETAILS

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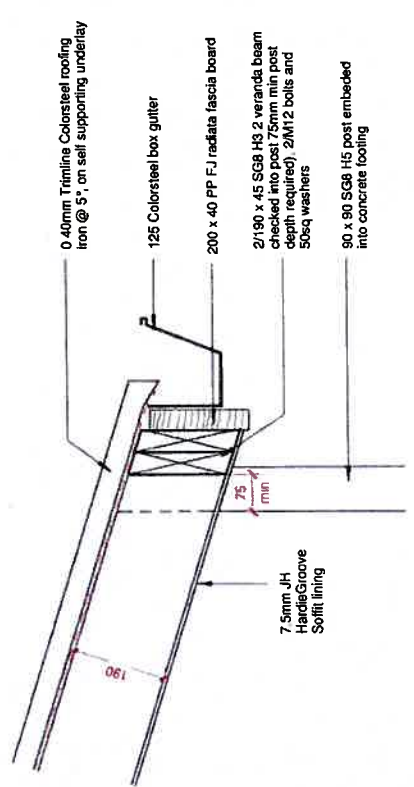
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A-504

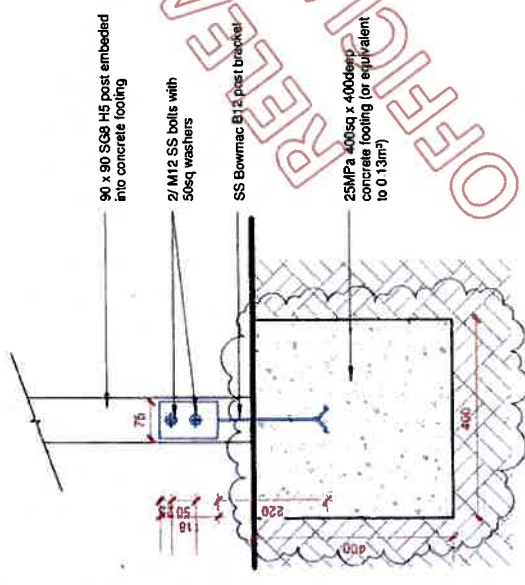
DATE PRINTED (19-10-17)

OFFICIAL INFORMATION ACT

OFFICIAL INFORMATION UNDER THE  
RELEASED INFORMATION ACT



01 VERANDA DETAIL  
1:10



02 VERANDA FOOTING  
1:10

DATE PRINTED (19-10-17)

Scale: 1:10

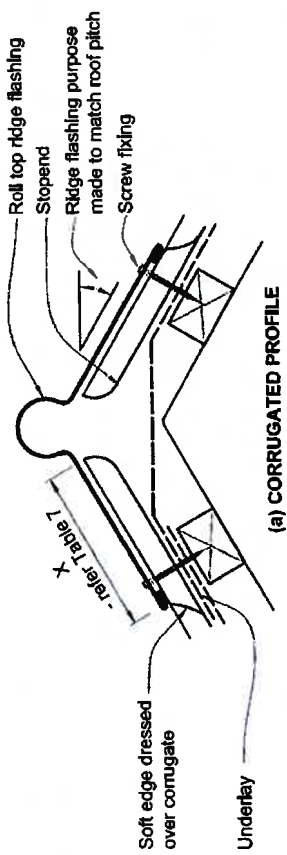
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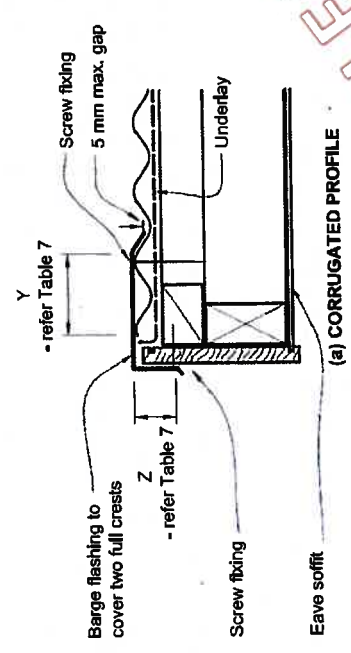
REVISIONS

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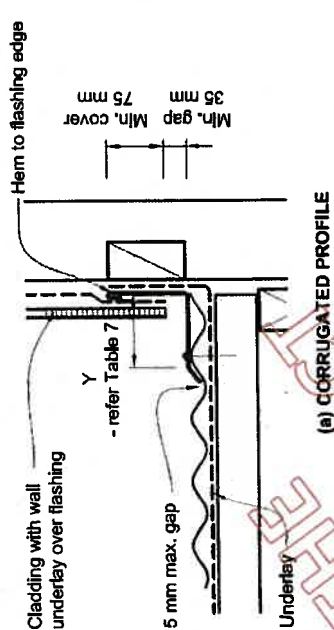
VERANDA DETAILS



(a) CORRUGATED PROFILE



(a) CORRUGATED PROFILE



(a) CORRUGATED PROFILE

OFFICIAL INFORMATION UNDER THE RELEASED INFORMATION ACT

### Demand Calculation Sheet

two storey

Job Details

Name: Russell Wharf Kiosk

Street and Number: Russell

Lot and DP Number: A.R.

City/Town/District: Arcline

Designer: 23/11/2017

Company Name: Russell Wharf Kiosk

Date: 23/11/2017

BRANZ Appraised  
Appraisal No 20: 1201:1

10 or 13 mm GIB® Plasterboard

Selected Lining Option

Building Specification

Number of storeys: two

Floor Loading: 2kPa

Foundation Type: slab

Cladding Weight: medium

Roof Weight: light

Room in Roof Space: no

Roof Pitch (degrees): 35

Roof height above eaves (m): 2.4

Building height to apex (m): 6.7

Ground to lower floor level (m): 0.3

Lower to upper floor level (m): 1.4

Slud Height (m): 12.0

Building Length (m): 8.5

Building Width (m): 104

Building Plan Area (m2): 104

Lower Floor: medium, light, no

Upper Floor: medium, light, no

check heights, storey less than 2 m

Building Location

Wind Zone: Very High

Select by Building Consent Authority Map or Preference: Very High

Wind Region: Preference sea 1

Lee Zone: Preference sea 1

Ground Roughness: Preference sea 1

Site Exposure: Preference sea 1

Topographic Class: Preference sea 1

Earthquake Zone: 1

Spit Type: 2SL (step to val)

Annual exceedance probability: 1/500 (NZS 3604:2011 d)

Bracing Units required for Wind

Demand W (BU)	along	across	Walls upper	Walls lower	upper
lower	1080	1452	540	717	
slab					431
slab				457	

Bracing Units required for Earthquake

Demand along / across E (BU)	Walls lower	Walls upper
slab	457	431

### SINGLE OR UPPER STOREY WALLS ALONG

Lines	1	2	3	4	5	6	7	8	9	10
Line Label	Line Label	Bracing Element No.	Available Wall Length L (m)	Angle to Bracing line (degrees)	Element Height H (m)	Bracing Type	Supplier	Bracing Units Achieved	Bracing Units Achieved	Bracing Units Achieved
263	A	1	0.7		2.4	Str 12H	JNL	74	74	88
		2	1		2.4	Str 12H	JNL	105	105	125
		3	0.8		2.4	Str 12H	JNL	84	84	100
252	B	1	2.4		2.4	none	JNL	252	252	300
		2	0.6		2.4	Str 12H	JNL	63	63	75
263	C	1	0.6		2.4	Str 12H	JNL	126	126	150
		2	1.2		2.4	Str 12H	JNL	74	74	88
		3	0.7		2.4	Str 12H	JNL			
Totals Achieved					126%	EQ		312%	777	925
Concrete Slab									OK	OK
Totals Required (from Demand)									619	296

### SINGLE OR UPPER STOREY WALLS ACROSS

Lines	1	2	3	4	5	6	7	8	9	10
Line Label	Line Label	Bracing Element No.	Available Wall Length L (m)	Angle to Bracing line (degrees)	Element Height H (m)	Bracing Type	Supplier	Bracing Units Achieved	Bracing Units Achieved	Bracing Units Achieved
260	M	1	0.9		2.4	BLP-H	GIB®	130	130	130
		2	0.9		2.4	BLP-H	GIB®	130	130	130
167	N	1	0.6		2.4	BLP-H	GIB®	83	83	83
		2	0.6		2.4	BLP-H	GIB®	83	83	83
300	O	1	2		2.4	BLP-H	GIB®	300	300	300
430	P	1	0.9		2.4	BLP-H	GIB®	130	130	130
		2	2		2.4	BLP-H	GIB®	300	300	300
Totals Achieved					104%	EQ		390%	1156	1156
Concrete Slab									OK	OK
Totals Required (from Demand)									1117	296

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Scale: Drawn By: Designer

Sheet No: A-601

REVISIONS

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BAY OF ISLANDS, FAR NORTH

BRACING CALCULATIONS  
GROUND FLOOR

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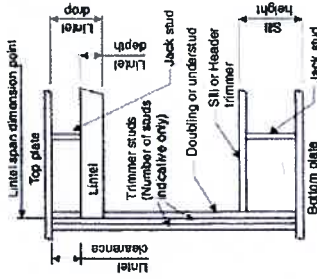
arcline  
architecture

## LINTEL FIXING SCHEDULE ALTERNATIVE TO TABLE 8.14 & FIGURE 8.12 NZS 3604:2011

**NOTE:**

- All fixings are designed for vertical loads only. Dead loads include the roof weight and standard ceiling weight of 0.20 kPa.
- Refer to Table 8.19 NZS 3604:2011 for rafter schedule to resist horizontal loads.
- These fixings assume the correct choice of rafter/truss to top plate connections have been made.
- All fixings assume bottom plate thickness of 45mm maximum. Note: TYLOR options on timber trusses are not covered in this schedule.
- Wall framing arrangements under girder trusses are as per NZS 3604:2011.

**DEFINITIONS**



**Lintel Supporting Girder Trusses:**

Roof Tributary Area	Light Roof		Heavy Roof	
	Wind Zone	Wind Zone	Wind Zone	Wind Zone
	L	M	L	M
0.6 m²	G	H	G	H
11.6 m²	G	H	G	H
12.1 m²	G	H	G	H
15.3 m²	H	H	G	H
19.1 m²	H	H	G	H
24.9 m²	H	H	G	H
27.8 m²	H	H	G	H
34.3 m²	H	H	H	H

- Notes:**
- Roof Tributary Area = approx. 1/2 x (Total roof area on girder and rafter trusses supported by lintel)
  - Assumed girder/truss is at mid-span or middle third span of lintel
  - Use similar fixings for both ends of lintel
  - All other cases require specific engineering design

**SELECTION CHART FOR LINTEL FIXING**

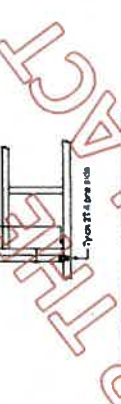
Lintel Span	Loaded Depth (mm)	Light Roof		Heavy Roof	
		Wind Zone	Wind Zone	Wind Zone	Wind Zone
		L	M	L	M
0.7	3.0	E	E	E	E
	4.0	E	E	E	E
0.9	3.0	E	E	E	E
	4.0	E	E	E	E
1.0	3.0	E	E	E	E
	4.0	E	E	E	E
1.2	3.0	E	E	E	E
	4.0	E	E	E	E
1.5	3.0	E	E	E	E
	4.0	E	E	E	E
2.0	3.0	E	E	E	E
	4.0	E	E	E	E
2.4	3.0	E	E	E	E
	4.0	E	E	E	E
3.0	3.0	E	E	E	E
	4.0	E	E	E	E
3.6	3.0	E	E	E	E
	4.0	E	E	E	E
4.2	3.0	E	E	E	E
	4.0	E	E	E	E
4.8	3.0	E	E	E	E
	4.0	E	E	E	E

**LINTEL FIXING OPTIONS**

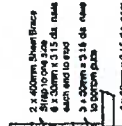
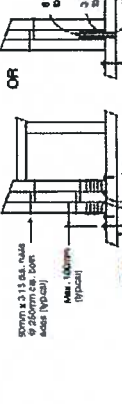
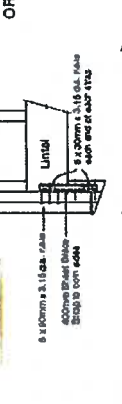
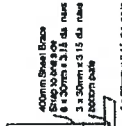
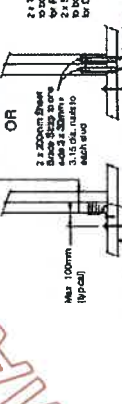
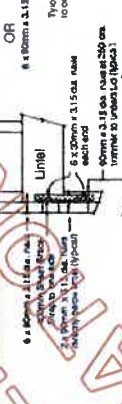
**TYPE E  
1.4RN**



**TYPE G  
7.5RN**



**TYPE H  
13.5 RN**



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 Auckland, New Zealand  
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**LINTEL FIXING SCHEDULE**

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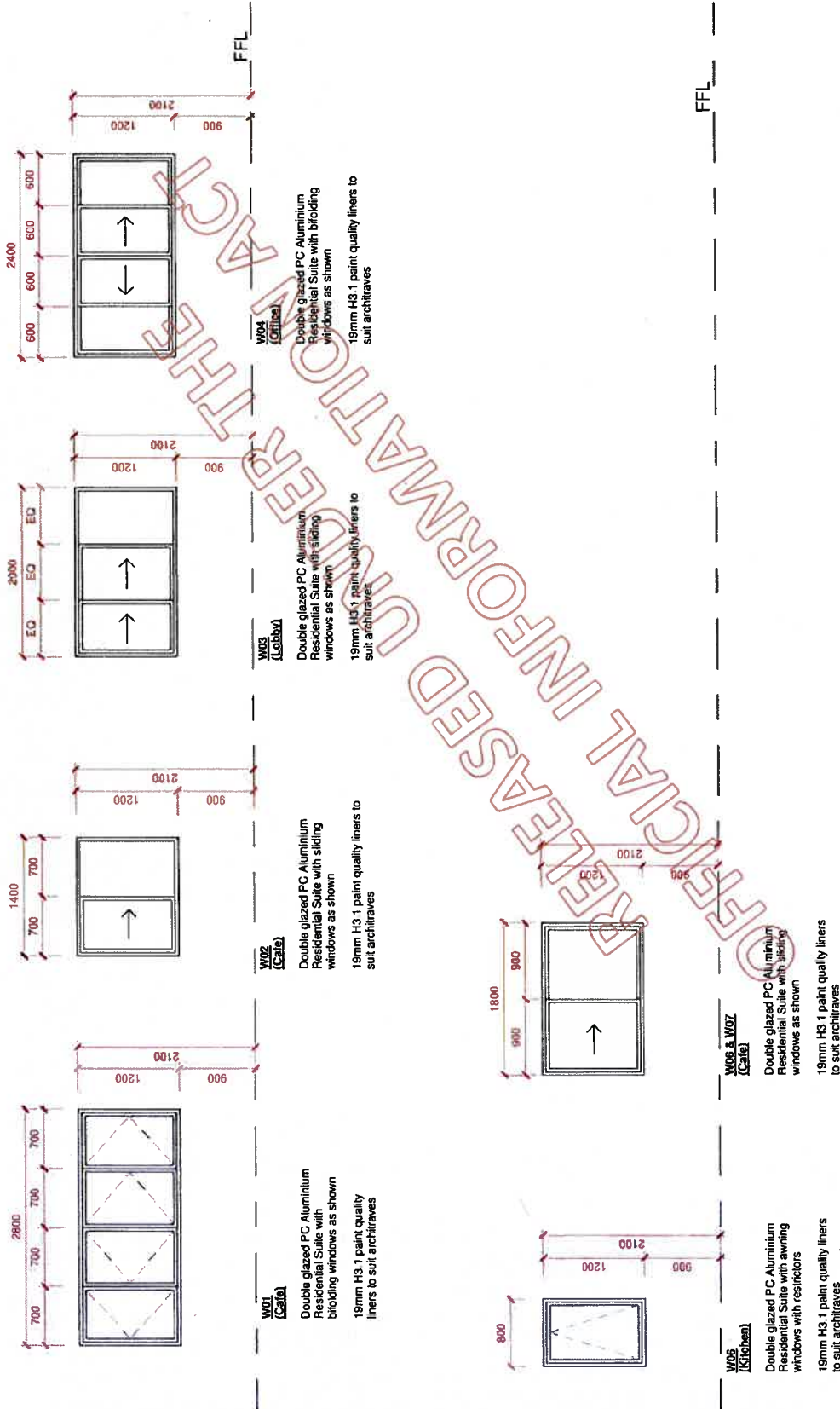
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**Note:**

- All joinery sizes are to finish sizes add 7.5mm each side for box (unit) size
- Refer to window manufacturers' schedule for trim opening requirements
- All dimensions are to be checked on site by joinery manufacturer prior to commencing manufacture
- Discrepancies are to be reported to the designer
- All fixings and fastenings to comply with NZS5604:2011 Part 4 'Durability and NZBC B2
- Windows installation, in accordance with WANZ recommended practice Return building wrap and provide air seals and flashings as per WANZ
- All windows to have restrictor stays and double snibs
- Sill support bars to be provided to all external windows & doors to comply with E2/AS1 and WANZ evaluation method EM6
- All safety glass and comply with NZ standards 4223:part 3
- Protecto wrap to all windows and doors
- All doors to have holdback stops



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Sheet No

A-603

Scale: 1:50

Drawn By: Designer

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RUSSELL WHARF / KIOSK  
 BAY OF ISLANDS, FAR NORTH

WINDOW SCHEDULE

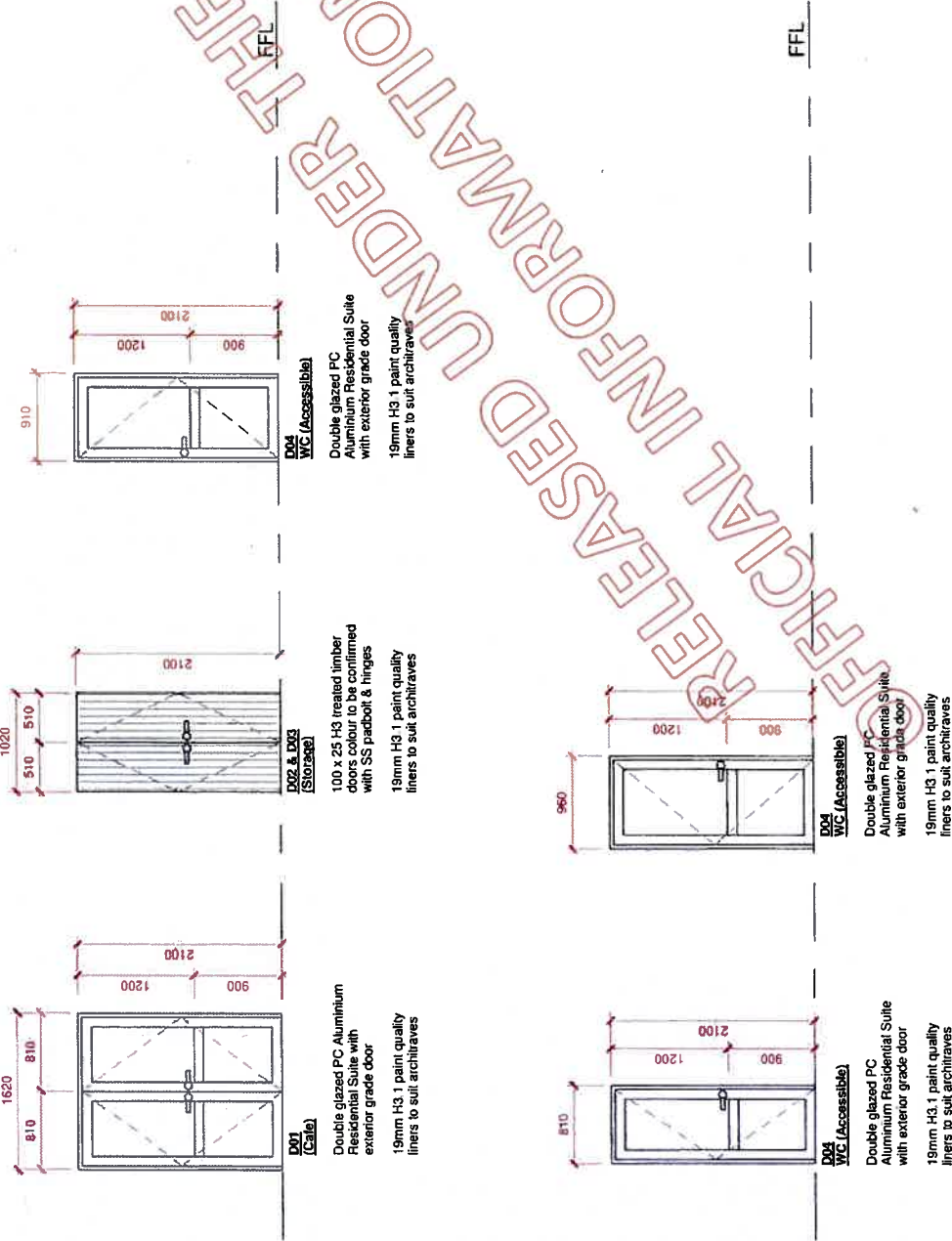
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**NOTE:**

- All joinery sizes are to finish sizes add 7.5mm each side for box (unit) size
- Refer to window manufacturers' schedule for trim opening requirements
- All dimensions are to be checked on site by joinery manufacturer prior to commencing manufacture
- Discrepancies are to be reported to the designer
- All fixings and fastenings to comply with NZS3904:2011 Part 4 'Durability and NZBC B2
- Windows installation, in accordance with WNZ recommended practice Return Building wrap and fastener seals and flashings as per WNZ.
- All windows to have restrictor slays and double sills
- Sill support bars to be provided to all external windows & doors to comply with E2/AS1 and WNZ evaluation method EM6
- All safety glass and comply with NZ standards 4223 part 3
- Protecto wrap to all windows and doors
- All doors to have holdback stops



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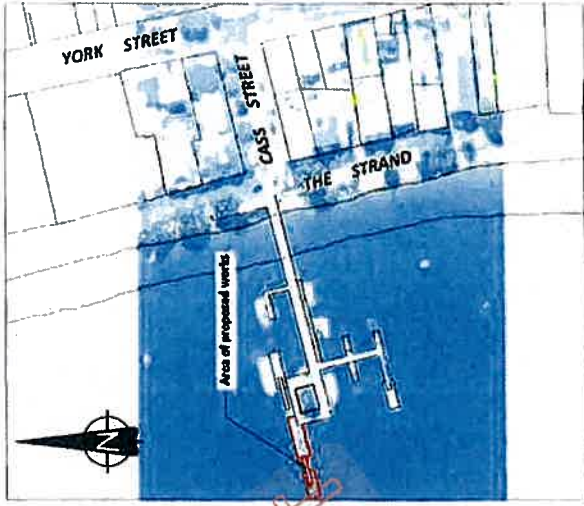
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 BAY OF ISLANDS, FAR NORTH

DOOR SCHEDULE

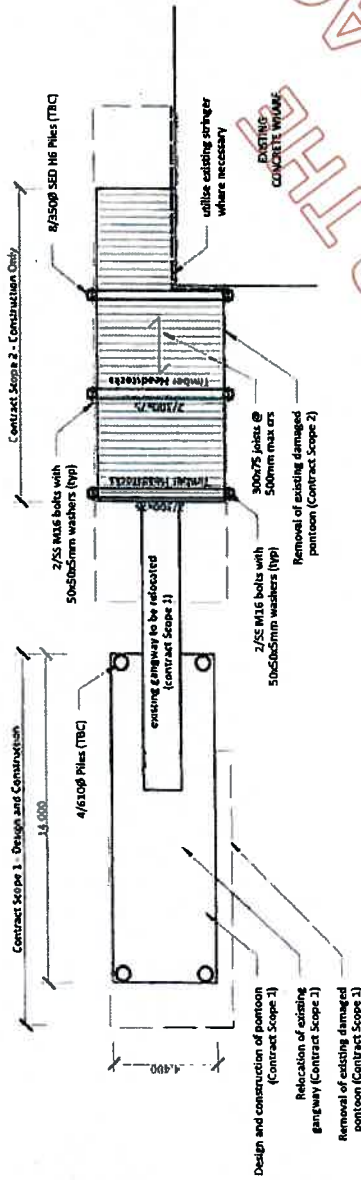
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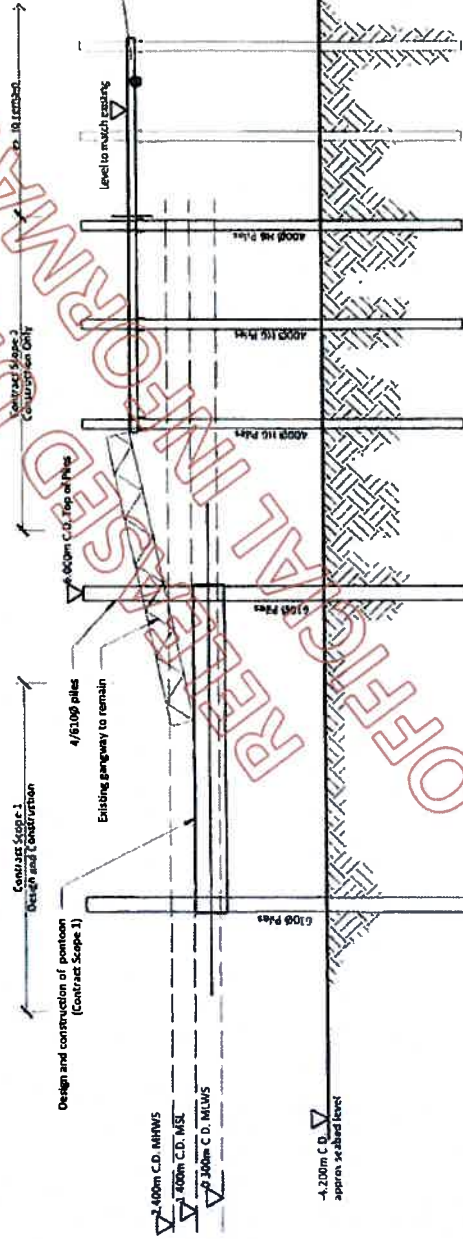


**PROPOSED WORKS - LOCATION PLAN**  
1:2,000

	<b>RS Eng Ltd</b> 09 438 3273 office@rseng.co.nz 2 Swanview Road, Whangarei 0110
	<b>PROPOSED JETTY REPLACEMENT</b> <b>Far North Holdings Limited</b>
<b>CLIENT</b> FAR NORTH HOLDINGS	<b>LOCATION</b> RUSSELL OUTER PONTOON RUSSELL TOWNSHIP
<b>DATE</b> 09/01/18	<b>ISSUE</b> A Original Issue
<b>SCALE</b> As Shown	<b>REVISIONS</b> Rev Original A3
<b>DRAWN BY</b> RNW	<b>APPROVED BY</b> MAX
<b>PROJECT NO.</b> 15627	<b>SHEET NO.</b> 1



**PROPOSED WORKS - PLAN**  
1:200



**PROPOSED WORKS - SECTION**  
1:200

- NOTES:**
- All services should be located on-site prior to commencement of works
  - All works to comply with all relevant local authority by-laws and council regulations where applicable
  - Contractors to confirm all dimensions on site prior to commencing any work
  - Do not scale off drawings.
  - These drawings are to be read in conjunction with specifications - plans take precedence
  - If any part of these documents are unclear, please contact RS Eng Ltd.
  - This plan is copyright to RS Eng Ltd and should not be reproduced without prior permission.

New Timber Jetty (Contract Scope 2)  
 150x50 decking on  
 300x75 joists @ 500mm max c/c on  
 2/52 M16 bolts with 50x50mm washers (typ)  
 4/400 SED 16 Piles

**RUSSELL WHARF DEVELOPMENT: ENGINEERS CONSTRUCTION COST ESTIMATE**

Prepared by:  
RS Eng Ltd



Date: 1/02/2018  
Client: Far North Holdings Ltd



Item	Description	Unit	Quantity	Rate	AMOUNT
<b>Schedule A - Investigations, Design, drawings, planning variations to existing Consent</b>					
1	Investigation, as built survey, seabed survey	LS	1	\$10,000	\$ 10,000
2	Structure design, drawings	LS	1	\$7,500	\$ 7,500
3	Tender Documentation and Tender Evaluation	LS	1	\$5,000	\$ 5,000
4	Planning and Variation to Resource Consent	LS	1	\$15,500	\$ 15,500
				<b>Sub total A:</b>	<b>\$ 38,000</b>
<b>Schedule B - Establishment, Insurances, Demolition</b>					
1	Site Establishment, Supervision and other Preliminary and General costs	LS	1	\$ 15,000	\$ 15,000
2	Mobilisation and demobilisation of plant piling gear, barge and dredging equipment	LS	1	\$ 20,000	\$ 20,000
3	Compliance with resource consent conditions	LS	1	\$ 1,000	\$ 1,000
4	Demolish of Existing step structures (P1, P2 and P3), include cut off piles	LS	1	\$ 32,000	\$ 32,000
				<b>Sub total B:</b>	<b>\$ 68,000</b>
<b>Schedule C - Construction</b>					
1	<b>Wharf Extension 15m x6m - West</b>				
1.1	Steel piles with PE sleeve	No.	8	\$ 3,750	\$ 30,000
1.2	Head stock and tie into existing wharf	LS	1	\$ 10,000	\$ 10,000
1.3	New concrete decking, pre-cast panels and topping	m <sup>2</sup>	80	\$ 1,350	\$ 108,000
1.4	New Handrail to side of wharf	m	27	\$ 750	\$ 20,250
2	<b>Pontoons</b>				
2.1	P1, P2 and P3: Concrete pontoons with 600mm freeboard, fender piles and new gangway	No.	3	\$ 135,000	\$ 405,000
2.2	Pontoon, gangway and platform extension for Area South of Main wharf	LS	1	\$ 150,000	\$ 150,000
2.3	Pontoon, gangway and platform extension for Area North of Main wharf	LS	1	\$ 125,000	\$ 125,000
3	<b>Fender Piles</b>				
3.1	New 400.5ED 14m H6 timber piles	No.	13	\$ 1,750	\$ 22,750
4	<b>Construction Supervision</b>				
4.1	Building Consent Inspections, Contract Admin, structural inspections and engineering sign off	LS	1	\$ 37,500	\$ 37,500
				<b>Sub total C:</b>	<b>\$ 908,500</b>
8	<b>Contingency, other items</b>	LS	1	\$100,000	\$100,000
				<b>TOTAL A + B + C:</b>	<b>\$ 1,114,500</b>
				<b>GST:</b>	<b>\$ 167,175</b>
				<b>GRAND TOTAL</b>	<b>\$ 1,281,675</b>



Russell Business Association,

P.O. Box 68,

Russell 0242

7<sup>th</sup> February 2018

To the Russell Wharf and Waterfront Trust.

The Russell Business Association wishes to acknowledge support for the proposal to upgrade, maintain and enhance the Russell Wharf.

As Russell is one of the prime destinations in the Bay of Islands experience, it is imperative that we have a robust working structure. The daily marine and commuter traffic relies very heavily on it's ability to support our business & community infrastructure.

For over 200 years the Russell Wharf has been the gateway to our Historic & Contemporary Village, where Game Boats, Sightseeing Adventures & Passenger Ferries embark & disembark the travelling public daily. The volume of Cruise Ship passengers to our shores are increasing and the need for maintenance & enhancement would greatly improve safety & aesthetics.

At present the Wharf structure has been compromised by the closure of the Northern pontoon which was due to be serviced in April this year. This closure at the peak of our Summer trade, has not only inconvenienced the Operators who use that facility, but shown that the Russell Wharf is under serviced and overlooked.

The Russell Wharf & Waterfront Trust have proven their agility & acumen as they interact & communicate with Far North Holdings to upgrade facilities that would only remain the poor cousin without funding.

The Russell Business Association supports the application in principle with consultation of the Operator/Users of the Russell Wharf.

Yours Sincerely

*Janet Planet*

Janet Planet

Chair

Russell Business Assoc.

Tel; 09 4037 269

07 February 2018

To whom it may concern

**LETTER OF SUPPORT FOR THE DEVELOPMENT OF PAIHIA AND RUSSELL WHARVES**

Fullers GreatSights is the largest cruise and tour operator in Northland, and is based in the Bay of Islands. The business has been operating in the Bay since 1887, and is a heavy user of the Russell and Paihia wharf infrastructure.

Fullers GreatSights has been proud to lead the promotion of the Bay of Islands, and in recent years has enjoyed significant growth, as they reap the rewards of this promotion, and the destination rebounds following the GFC. This growth has led to a large investment in new plant over the last two years.

Unfortunately, the investment in infrastructure has not kept up with the increase in demand and vessel traffic, and Fullers GreatSights now find themselves being constrained by the wharf infrastructure. The wharves are in a bad state of repair, are insufficient for peak requirements and are often below an acceptable safety standard. The recent decommissioning of the primary pontoon on Russell Wharf, during the busiest week of the year, would be the perfect example of this.

Fullers GreatSights have been working with Far North Holdings to prioritise improvements to the wharves, as it has been identified as a key area for improving Fullers GreatSights's customer experience, but are very concerned with the slow progress of this work, and the continued deterioration of the structures.

This proposal therefore has the complete support of Fullers GreatSights as it will ensure that the business is able to continue to grow and invest with confidence.

Yours sincerely

A handwritten signature in blue ink, appearing to read 'C Parker'.

**Charles Parker  
General Manager BOI**



Where Stories Begin

07 February 2018

To whom it may concern

**LETTER OF SUPPORT FOR THE DEVELOPMENT OF PAIHIA AND RUSSELL WHARVES**

The Bay of Islands Marketing Group consists of ten industry operators who have invested well over \$1 million in marketing the Bay of Islands in the last five years.

As a group we understand the importance of the destination presenting a professional and safe image, and an important component of the destination's image is the resident infrastructure. For this reason the group is very supportive of the efforts by Far North Holdings Ltd to redevelop the Paihia and Russell Wharves.

The existing wharves have not kept up with the growth in demand by operators and this has led to congestion on, and delays in accessing, the existing wharf structures. This has led to increased cost for operators and compromised customer experience for all wharf users.

In addition, the existing wharf structures are known to be in a poor state of repair, on occasion unsafe and regularly fall under normal usage. The recent urgent removal of the main cruise vessel pontoon in Russell, during the busiest week of the year, is confirmation that the structures are falling apart.

If the wharves are not repaired urgently the chances of a catastrophic failure will continue to increase, and the Bay of Islands Marketing Group, and the region can little afford the reputational damage this would cause.

In summary, the Bay of Islands Marketing Group is very enthusiastic about the redevelopment of the Paihia and Russell Wharves. We look forward to the customer experience improvements that will follow.

Yours sincerely

A handwritten signature in blue ink, appearing to read "Charles Parker".

Charles Parker  
Chair of the Bay of Islands Marketing Group





08 February 2018

Attn: Chris Galbraith  
Far North Holdings Limited  
Opuā

To Whom It May Concern

**RE: Paihia and Russell wharf developments**

I am writing on behalf of Business Paihia Inc. in support of the Paihia and Russell wharf developments proposed by Far North Holdings Ltd (FNHL).

Through our long-established relationship with FNHL and commercial wharf users, we believe the development to be well overdue. The wharves in question are no longer fit for purpose as current facilities have deteriorated over time with use, visitor numbers (and therefore wharf users) are increasing every year and commercial maritime fleets have expanded. Safety, access and quality of experience for both locals and visitors alike will benefit from the development of the Paihia and Russell wharves.

The proposed wharf development project would also encourage local businesses (tourism, transport, retail, hospitality and various other service industries) to invest in their own maintenance and expansion, resulting in a positive outcome for the local economy, increased employment and an improved visitor/user experience.

Business Paihia Inc. are in full support of the proposed Paihia and Russell wharf development plans and look forward to supporting further FNHL projects that will encourage growth of the Bay of Islands commercial and visitor economy.

Kind regards

A handwritten signature in blue ink, appearing to read "Tania Burt".

Tania Burt

**Secretary**

**Business Paihia Inc.**

E: [taniaburt@waitangi.org.nz](mailto:taniaburt@waitangi.org.nz)

P: 09 402 7437 ext. 243

## Chris Galbraith

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**From:** Ross Brljevic <ross@exploregroup.co.nz>  
**Sent:** Thursday, 8 February 2018 9:07 a.m.  
**To:** Chris Galbraith  
**Subject:** FW: FNHL Letter

To Whom it May Concern,

I write this letter in support of FNHL proposed developments for both the Russell and Paihia Wharf's

Over recent years the number of tourist have increased substantially to Northland but unfortunately the infrastructure has not kept pace.

The current wharf set ups at both Russell and Paihia , do not meet the demands of an increasing tourist and local markets. The recent removal of a pontoon in Russell has created chaos for all commercial and domestic users. With the increased numbers of Cruise Ships arriving , the visitor experience is not acceptable. We have had several of our scheduled trips , held up purely due to the fact that the infrastructure cannot cope with the demand.

FNHL have been proactive in their approach to fixing the numerous issues and we as one of the larger commercial operators in the Bay of Islands support their efforts.

By no means detracting from our support but at the appropriate stage we would like to see a dredging plan as know there is substantial sediment movement around the wharf and with the changed vessel movements the impacts should be understood, we are happy to provide further specific detail when required.

Explore is looking to increase the size of their business offering in the coming years and need the infrastructure working and increasing for this to happen.

Any support that MBIE might offer to assist this happening will be greatly appreciated.

Ross Brljevic



**Russell Ratepayers & Citizens Association Inc.**  
**PO Box 57**  
**Russell 0202**

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7 February, 2018

TO THE RUSSELL WHARF AND WATERFRONT TRUST

We have viewed Far North Holdings planned improvements for the Russell Wharf and are thrilled an opportunity has arisen to apply for finance to implement long overdue alterations and additions.

These proposed improvements will:

- Provide a safer, fit for purpose, berthing for commercial and private boats
- Provide a safer and regulated area for our kids who love jumping off the wharf
- Provide an enlarged i-Site facility for the increasing numbers of tourists needing guidance and information.
- Provide additional, user friendly berths to handle the increasing numbers of boat using the Russell Wharf.

Russell Wharf has traditionally been the 'Gateway' to our town, so the proposed improvements will benefit businesses and enhance our visiting tourists' experience.

We wholeheartedly support your efforts to seek funds to upgrade Russell Wharf and wish you every success.

Regards,

Ray Ebbett  
Chair



Wednesday 7 February 2018

To Whom It May Concern:

I write in support of the Russell Wharf improvements, which are long overdue. Our community has been requesting it for many years.

As users of the wharf, Russell School, supports the improvements for many reasons.

Our "iconic" WHARF SPORTS have been held for probably 80 to 90 years. Wish we knew when they started!

Each year, children from Russell, Paihia, Opuia, Waikare, Oromahoe, Whangaruru, and Maromaku swim against each other in the sea at Russell Beach. An oyster barge is tied up along the lower wharf boardwalk. A floating pontoon along the wharf would certainly make it easier for the children to dive off into the swimming lanes.

We also use the wharf when children are training for their 100m, 200m, 400m, 800m and 1000m swims.

Bombing off the wharf is also iconic both for locals and tourists alike. When our children visit Wellington city and see the designated area and equipment for bombing, they come back wanting one here! Why not!

On a personal note, I was returning from a cruise to Urupukapuka Island earlier this year, on arrival there were at least 20 children being supervised by their parents, bombing off the wharf. What a great thing for families to do. I noted that none of the children were locals. I took this as an indication that both locals and tourists would agree with a designated area for bombing. Hopefully equipment too!

Russell School strongly supports the improvements being made at the wharf, and the sooner the better!

*W Fuller*

William Fuller  
Principal

## Chris Galbraith

---

**From:** Riki Kinnaird <riki@theduke.co.nz>  
**Sent:** Wednesday, 7 February 2018 1:45 p.m.  
**To:** Chris Galbraith  
**Subject:** Russell Wharf and Waterfront Trust - Email Support for FNH

To Chris Galbraith

The Russell Wharf and waterfront Trust is a charitable trust that works with the council, Far North Holdings and the community to improve and protect the Russell marine assets and its environment. We also work strategically to ensure our assets are fit for purpose in the future.

We whole heartedly support the work that Far North Holdings are completing to develop The Russell Wharf. The Russell Wharf is run down and not fit for purpose, it is also dangerous and susceptible to damage from storms.

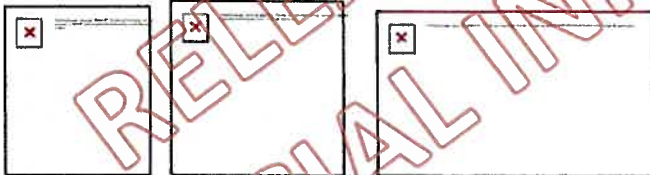
The wharf is important to Russell as it is our main highway for tourists, staff and the community. It has been the centre of Russell activities for over 200 years including the school swimming sports. It should showcase what this region has to offer and form part of our offering (not hinder it).

It is not good enough that this key asset in a growing environment is not for purpose.

If you need anything else please call me on 021983531.

Regards Riki

**Riki Kinnaird**  
Duke  
ph +64 09 4037829  
mobile +64 21983531  
35 The Strand, Russell, New Zealand  
[www.theduke.co.nz](http://www.theduke.co.nz)  
[www.facebook.com/TheDukeRussell](https://www.facebook.com/TheDukeRussell)



## RUSSELL SCHOOL ESSAYS

Persuasive arguments  
from Russell School pupils

Pupils at Russell School have to learn how to put a persuasive argument. Can they grab your attention, convince you with facts and evidence, summarise their view and suggest action? This year's Russell Review Essay gives a practical expression to those skills.

The signs on the Russell Wharf say that no-one is allowed to swim or dive off the wharf, but sometimes on hot days, any time of the year, people see children, teenagers and sometimes even adults diving there. Should they be allowed to do this? The signs say NO! There is a floating pontoon close to the Russell beachfront which any one can use but still children/teenagers seem to prefer to dive/jump off the wharf.

Years 7 and 8 pupils at Russell this year faced the assignment that their persuasive argument essays state their point of view on 'Children and adults should not jump or dive off the Russell wharf - the signs say NO!'

Assisted by their teachers Evelyn Pausma and Lyn McMillan, the children made their cases. After some difficulty, because the standard of presentation was high, the following four winners were chosen, two from each year.

Congratulations to the rest, they were excellent too.

off the wharf in Auckland city. It's far too dangerous and there are too many boats.

So why can't we jump off the Russell wharf? It's unique. In most places in the world it's far too dangerous to go jumping off the wharf.

So I believe that having great times with mates and entertaining the tourists far outweighs following the rules of a sign. It's priceless fun.

By Eamonn Magee

Year 8

### Get rid of those signs

Why are we not allowed to jump off the wharf? It's been done for generations, so why should we stop having great times? Who do they think they are?

When I first came to Russell School I found out very quickly from the children I met that the highlight of the afternoon in the summer was jumping off the Russell Wharf, and that the wharf was the best place to have fun. I have never noticed the signs before, so why should I stop now?

Who put up the signs in the first place? I'll tell you who, the Northland Regional Council. They're the ones who want us kids to give up our after school fun. I believe that yes, children and teenagers should be allowed to jump off the Russell wharf.

Tourists have a ball watching the kids jump and dive off the wharf. It's a real novelty to those who came from the big cities. There would be no way that children would be allowed to jump off city wharves. I don't think children jump

has been jumping off the wharf since she was little. She still jumps off the wharf sometimes. It is essential that children will be able to jump off the wharf. It is a Russell tradition.

Why sit at home watching TV all day when it is so sunny and the water is so blue? You don't get any exercise. It is an excellent idea to run down to the wharf and go for a swim. You get exercise and you can have fun at the same time. You will thank me if you do this.

If you have some friends or family coming to see you, there is no reason they shouldn't go for a swim off the Russell



Should people be allowed to swim, dive and jump off the Russell wharf?

Nothing is more refreshing than a beautiful dive off the Russell wharf in the hot February days.

I believe that people must be allowed to swim, dive and jump off the wharf. In all the time I have been jumping off the wharf I have never heard of anyone who has died off the Russell wharf. I have been jumping off the wharf since I was little.

My papa used to jump off the wharf when he was young and his daughter, my mum,

wharf. All you need to tell them to bring is togs and a towel, you don't need anything else. It is simple.

Lastly, if you go for a swim off the Russell wharf you can tell your children, grandchildren and your great grandchildren that you swam off that wharf.

Steffi Hooson  
Year 7

Judge's comments:

The judge was in complete sympathy with those who argued in favour of jumping/diving off the wharf, having experienced that pleasurable activity many years ago in various places including Russell. However, the 'march of time' has made this activity impractical with the increased use by commercial boats and the flow of tourists using the wharf.

Why not approach the Northland Regional Council, show them your arguments and ask for an upgraded pontoon complete with a slide and elevated diving board?

I did enjoy reading the arguments; the standard of written expression was high. In particular it was great to read some inventive ideas to increase the pleasurable use of the pontoon. The freedom of expression was also very refreshing!! Good stuff!!

Joan Pasche

Thanks to Craig Holloway, ARDreas,  
for awesome photograph. Craig is now  
domiciled in Australia. We miss you Craig.

## The Wharves of Russell – 1845 Onwards

by *Barrie Bell*

Source:  
Russell  
Review 2011-2012

Yes, we have had two new wharves the last 138 years and that is not counting the numerous upgrades, renovations and repairs – and renovations are what we are about to enter into again (we hope) with a new refurbishing program waiting in the wings... It should also be said that, in their time, these renovations and upgrades have been both expensive and controversial and retaining a functional wharf has been a hard fought battle.

Why has the wharf been so important to the village of Russell? Why the controversies? Why have the rebuilds, the extensions, and the alterations all gone ahead in spite of the objections from some quarters? Perhaps by looking at the 'wharf things' historically a clearer picture may emerge.

Firstly, let us look at the population of Russell: in 1840, when Governor Hobson visited Russell (Kororareka), the population was listed officially as being in excess of 600 and probably nearer to 1,000. The total European population of New Zealand at that time was only a little more than 2,000, so Russell was a very important place.

In 1851 the European population of Russell was a little over 400. With the Maori included the population must have still been well in excess of 600 and up towards the 1,000 mark. It seems that the numbers of people in the town remained this way until the Capital was moved to Auckland.

At the time of the sacking of Kororareka in 1845 there were no wharves as such in the Bay, but a local correspondent writing in May 1873, reported that he had seen plans for a new Government Wharf in Russell – this was particularly significant since the accommodation of shipping at this time was on two private jetties between the current Duke of Marlborough Hotel and the boat ramp, as can be seen in early photographs of the area.



These however were only able to accommodate small crafts due to the increasing draft of ships being built at the time and the tidal range in our harbour made their continued use a problem.

The size of Russell in the 1840s however this was only one of the reasons for the Government to regard the town as very important, another was the knowledge that

## Russell Review

Russell was an iconic town and birthplace of the Colony. Consequently, the Government decided to fund a public wharf in the Port of Russell.

At this time we were still the port of call for South Pacific whalers bringing large quantities of whale oil in for shipment. There are reports of up to 60 of these ships being anchored in the Bay while their crews enjoyed the local hospitality. However, it may be that those reporting on this sea of sails may also have been visiting one or more of the 20 local grog shops since official records show that the largest number of these ships ever in the Bay at one time was 45. Regardless of this, be the number 45 or 60, imagine the majesty of such a spectacle.

In September 1874, the Northern Luminary reported that 'a contractor has commenced work on the public wharf' (the new government wharf) and in 1875 a correspondent wrote 'the wharf accommodation is good'. It was being used at this stage by the SS *Iona* to ship oysters between Russell and Auckland. Whether or not the wharf was complete at the time is not clear from the documents but it must have at least been operational.

It should be noted that the Port of Russell covers a substantial area and an 'Order in Council of the 4th July 1872' shows the extent of this:

*'The Port and Harbour of the Bay of Islands is called Russell'* and this was first declared in 1840 and the boundaries and Port limits were appointed by the Governor, Sir George Frederick Bowen. In this declaration the Port was designated to cover an area enclosed by a circle of 2 nautical miles radius with its centre at Kororareka Point.

From this it can be seen that it would include the points at both Te Wahapu to Tapeka and probably Waitangi and Paihia too.

The new wharf however ran into some of the problems that were inherent in the old jetties, it was too short and ships (getting larger all the time) either couldn't get in at low tide, ran aground in the process or were left high and dry. In July 1880 extensions to the wharf were proposed at a cost of 600 pounds for extension and 100 pounds for access road works. 'And then the fight began,' (the words I saw in a recent funny email I received.) The Northern Luminary of 21st August 1880, reported that some residents outside Russell felt aggrieved that so much money was being spent on a wharf in this town and believed that it was a waste since the railway to Opua, a planned road from Auckland, and the consequent demise of the coastal shipping trade, would make the wharf extensions completely unnecessary. This argument became very heated and in a charade which some claimed was based on the Ned Kelly legend in Australia, the opponents of the scheme paraded an effigy of the County Council Chairman, Mr Henry Williams, along the beachfront at Russell and onto the wharf with appropriate belittling calls filling the air. At the end of the wharf the effigy 'was stabbed through the heart, burnt and kicked off the end and into the sea' whilst chanting, "give us back the 600 pounds for the wharf." In alternative version of this story another local resident said that the charade was based on the Charge of the Light Brigade rather than some reference to Ned Kelly. Some of the words given below seem to be consistent with this view of events and would match the chanting:

*"Half asleep, half asleep,  
Half asleep, " No Sirs,"  
All in the valley of Paihia  
Sat the Russell Councillors.  
"Forward the County Brigade!  
Charge for the money," one said  
"Russell wharf extension; Cheer!  
"Twill cost Six Hundred."*

(There are a number of other verses which I'm not reporting here.)

Fortunately, whichever version you prefer, the end result is the same and of course, as history shows us, the County Council approved the scheme and on March 5th 1881 tenders were called for the extension and improvements to the Russell Wharf and this process was underway during 1882.

In the meantime however there were significant changes occurring in the commercial life of Russell which went a long way to vindicating the actions of the County Council. Much earlier gold and coal had been found in the area but now other minerals were being detected, Manganese, quicksilver (mercury), silver and antimony. Along with some agricultural products and timber most of these new items were shipped from the Port of Russell and of course, many of the provisions used by our populace also came to us via our Port. At times the actual date of some of these occurrences is a 'little variable' but regardless of this it is clear that Russell was the most important Port in the area having comparatively deep water which did not require dredging – just be aware of the tide.

The way the people lived in Russell also receives favourable mention, an official visitor from the New South Wales colony reporting that Russellites had 'general good health', which he put down to their fish diet, but it may be amusing to note that another reason given for the health of the community was of a more psychological nature; speaking of Russell he wrote:

*"...the appearance is sleepy and the inhabitants take things very easy." Have we changed?*

This refurbished wharf lasted into the 1920s by which time it had reached a stage of being dangerous and had to be rebuilt. The road to Auckland was still incomplete and the arguments used, primarily the same as used by the opponents of the upgrade of 1880, were difficult to sustain as sea transport to the Bay was still the only viable option.



An interesting insight into the significance of the wharf to our visitors of in the 1930s is emphasized in an article in 'The Bungalow' written in this period: *"We left Auckland at 8.25am and got to Russell at 7.30pm – the last part of the trip by launch."*

How long this must have taken before the launch trip with the road to Auckland being incomplete and often impassable in places.

Of course, no discussion of early Russell would be complete without mentioning The Northern Steamship's *Clansman* which was the main contact with the outside world for mail, provisions and passengers and for taking local produce back to Auckland. The *Clansman* provided this service from 1884 to 1931, by which time the service was no longer economically viable as road and railway had taken over as had been predicted 50 years earlier. The *Clansman* made this trip from Auckland to the Bay twice weekly and tales of this boat are legend. One which appealed to me was reported in this way:

*"...on a foggy morning with visibility nil, the old ships foghorn could be heard miles away, sounding long blasts which would be answered by some of the inhabitants of Russell lusterley hammering four gallon kerosene tins to indicate the whereabouts of the wharf."*



## Russell Review



But by now the purpose of the wharf had changed significantly. There was an increasing tourist trade needing ferries to convey people to and from Russell. There was a large commercial fishing fleet operating out of the Bay, and as well as big game fishing, made famous by Zane Grey, attracted fisherman from all over the world.

With engineering reports in the 1980s recommending the demolition of the wharf on safety grounds the issue was once again on the agenda and its new uses couldn't be ignored.

On the 25th May 1982 the Northland Harbor Board, as it was now known, met with the Russell Community Council in The Duke of Marlborough Hotel to consider proposals for a new but smaller wharf. The 'smaller' idea met with a deal of local opposition but given the new needs of a wharf with the large coastal steamers no longer operating, the 'smaller' idea prevailed.

And now, once again we are in the midst of an upgrade proposal but this time most of the arguments are primarily commercial. A number of local residents and business owners saw the wharf deteriorating and began

thinking of a process of 'fixing it up' before it reached a dangerous condition. A lot of businesses in Russell depend on a lively tourist market for their viability and with tourism increasing, especially now with larger numbers of cruise ships overnighing in the Bay, our wharf must be kept in sound condition or we will lose this trade to Paihia and Waitangi - we must continue to have a share of this trade which benefits us all.

On the 15th October 2010 a public meeting in Russell approved the idea of significant renovations being made to the wharf and drawings of a proposal were made public for community consideration. On the 27th April this year these plans were approved by a majority of members of the Far North District Council. The financial implications of this are as usual very significant for our small community but the Council has agreed to contribute \$367,000 and the remaining \$100,000 needed is our responsibility. Easing this burden somewhat is an agreement with the FNDC that the \$100,000 is to be provided by them as an interest free loan over a period of two years. There are some other bonuses in this process: We will see important improvements to the boat ramp and hopefully the parking in the area, and also get a playground for our children. This process is now awaiting resource consent.

Diving, swimming and fishing are supposedly a no-no from our wharf, but when it is fully refurbished this will continue to be an unenforced 'law' and our children of now and I hope many generations to come, will still be able to enjoy these recreations and the life skills they impart.

# Russell Lights



ISSN 174-5509

Volume 16 Issue 9 - \$2.50

**NO SWIMMING  
OR DIVING  
OFF THIS WHARF**

By Order: Northland Regional Council  
N.Z.

CIRCULATION  
Russell District Residents and Ratepayers

Thursday, 2 May 2010

## To Jump or not to Jump

*Russell children have been jumping off the wharf probably for as long as there has been a wharf. But should we allow this age old practice to go on with its dangers and a very obvious sign indicating the after-school pastime is not strictly allowed? Russell Lights asked two of our local students for their views, one for and one against.*

Should anyone be allowed to jump off the Russell Wharf? I oppose jumping off the wharf. Anyone can but are we allowed to?

When you walk along the wharf what do you see? You see a bright blue sign with a red cross over a picture of someone in the water. If you are too blonde to realise what this means it means - no jumping off this wharf! And you don't need to jump off the wharf, there is a pontoon positioned 20 metres away. Seriously, start thinking! Imagine being a tourist newly arrived in the village. You have just got off the Mini Tours bus and are wandering down the wharf to catch the foot ferry, taking in the stunningly beautiful views as you go. Then splat! You think perhaps it has started drizzling, but no, these are drops of saltwater thrown up by some kids having a bombing competition. You'd suddenly be shaken out of your wonderful daydream.

Swimming off the wharf is highly dangerous for many reasons. Some include sharp oysters shells that will give you deep gashes, wet surfaces that you could slip on, boats to swim into and multiple fishing lines to get tangled up with.

That leads me onto fishing. If you are jumping off the wharf you are disturbing the fish. If you are disturbing the fish then they will swim away. If the fish swim away there will be no fish around the wharf. If there are no fish around the wharf there will be no fish to catch off the wharf. I'm sure you know what that means. There is no point fishing off the wharf!

I am also pretty sure it is unhealthy to swim off the wharf. With all the petrol from the boats leaking into the water not to mention the fishy leftovers chucked over the side of the icing boats it is not

at all an eco-friendly environment. Swimming in that is disgusting, YUCK!

Whilst jumping off the Russell Wharf you are getting no life skills at all. What you could be doing is something productive like music lessons and art lessons. Fitness activities like soccer, hockey and netball. Do you agree?

When I looked up the definition of the word "wharf" in the dictionary, this is what I found: "a level quayside area to which a ship may be moored to load or unload." Not swim. It is a place for boats.

Should we jump off the wharf? Anyone can. But we are not allowed.

*Lucia Avery*

Why shouldn't we be able to jump off the wharf? The tourists love it and our parents have been doing it for generations. If you don't agree, good luck stopping me and all the other kids that do it.

I have been living in Russell for eight years now and before that we used to come out here on holidays and I used to love watching kids jumping off the wharf. So when my mum told me we were moving to New Zealand the first thing that hit my mind was jumping off the wharf. I took my first jump when I was just four years old. I am almost 13 now, and I am still doing it and have no plans to stop.

Almost every day I finish school and head straight for the wharf to cool down and have a swim with my friends. I have no idea why we should stop. You might say it is dangerous but how is it dangerous? The only way it's dangerous is if people start being idiots around the boats.

Sure people can get quite badly hurt, but isn't that just part of being a Kiwi kid? Plus aren't you better off jumping off the wharf than being an obnoxious little ... person smoking and doing graffiti around town?

So, will you people please give me one good reason why we shouldn't jump off the wharf? Otherwise I'm not stopping.

*Philippe Ebert*

**Crusieships Statistics**

## Bay of Islands

Year	No. of Ships visits	No. of Passengers	NZ rank
2008-09	37	25,704	
2009-10	32	36,138	
2010-11	36	45,316	
2011-12 World Cup year	48	63,503	5th=
2012-13	36	53,616	3rd =
2013-14	44	72,638	2nd
2014-15	43	63,981	
2015-16	54	108,978	
2016-17	51	92,223	
2017-18	63	119,000	
2018-19	72	142,000	

from 2016 to 2017 ye 23% increase in ship numbers  
30% increase in passenger numbers

from 2017 to 2018 ye 14% increase in ship numbers  
19% increase in passenger numbers

### **Cruiseship Community benefits**

Looking at the recent visit of the "Ovation of the Seas" on the 7<sup>th</sup> Feb, where new records were set for both the Bay and for New Zealand with 1,171 passengers on Shorex tours, (Tours sold on the ship) and in total over 4,000 passengers and crew coming ashore.

The shorex program for the visit included

Hole in the Rock, Cape Brett two cruise (196 passengers),  
Lunch cruises, (89 passengers),  
Glow Worm Caves (Kawiti), and Kauri Forest, Puketi (424 passengers),  
Island and caves Adventure, (2 x boat trips 67 passengers),  
Hokianga Giant Sand dunes boarding, (33 passengers),  
BOI Nature cruise and paddle boarding, (34 passengers),  
Historical Hokianga tour (70 passengers),  
Butler Point (Mangonui) Whaling museum and gardens, (41 passengers),  
Cycle tours (20 passengers),  
Historic Treaty House (Waitangi Treaty Grounds) and Pioneer settlement (Waimate North), (90 passengers),  
Coastal Sea Kayaking (48 passengers)  
Northland Maori Waka Encounter (59 passengers)

In addition to those on the above tours some 2,800 passengers undertook similar activities in Paihia including:

- Hole in the Rock, outer Bay of Islands tours, and Otehei Bay tours
- Ferry trips to Russell
- Helicopter trips
- Local tour operators to similar destinations as Shorex program above

In addition is the various shopping and eating opportunities within the Paihia and Russell areas, and it was noted that most passengers did not return till late in afternoon suggesting they will have likely spent monies on such activities.

The passenger spread across the district is very wide, from Mangonui in the north to the Hokianga in the west through to Kawiti Caves in the south and then across to Russell and areas in the east. Thus the economic benefits from the cruiseship passengers are widely spread amongst the Far North community.

BUS #	TOUR CODE	Tour Name	TOTAL	Meeting Time	Departure Time	Return Time	DUR	Meeting Location	ESCORTS	Crew ID
	BI12D	Glow Worm Caves and Kauri Forest	42	7:45 AM	8:30 AM	12:30 PM	4:00	Royal Theater, deck 4 FWD	20	42
1	42								Zeliko	818607
	BI19B	Bay of Islands Lunch Cruise	44	7:45 AM	8:30 AM	12:00 PM	3:30	Royal Theater, deck 4 FWD	10	45
2	44									
	BI16A	A Cruise to Zane Greys Hole in the Rock	100	7:45 AM	8:30 AM	11:30 AM	3:00	Royal Theatre, Deck 4 FWD.	70	100
3	100								Remus	42312
	BI12E	Glow Worm Caves and Kauri Forest	41	8:15 AM	9:00 AM	1:00 PM	4:00	Royal Theater, deck 4 FWD	20	42
4	41								Christopher	877461
	BI37A	Island and Cave Adventure	33	8:15 AM	9:00 AM	11:00 AM	2:00	Royal Theater, Deck 4 Fwd	15	34
5	33								Carlos	845234
	BI11A	Hokianga Giant Dunes Sandboarding	33	9:15 AM	10:00 AM	5:30 PM	7:30	Royal Theater, Deck 4 FWD.	20	40
6	33									
	BI39A	Bay of Islands Nature Cruise and Paddleboarding	18	9:15 AM	10:00 AM	1:00 PM	3:00	Royal Theatre, Deck 4 FWD	10	18
7	18									
	BI01A	Historic Hokianga	70	9:30 AM	10:15 AM	6:15 PM	7:00	Royal Theatre, Deck 4 Forward	20	70
8	36								Luigi	957618
9	34								Li Jin	972046
	BI41A	Butlers Point Whaling Museum and Gardens	41	9:45 AM	10:30 AM	3:30 PM	5:00	Royal Theater, Deck 4 FWD.	20	40
10	41									
	BI12A	Glow Worm Caves and Kauri Forest	85	9:45 AM	10:30 AM	2:30 PM	4:00	Royal Theatre, Deck 4 FWD.	20	86
11	43								Briana	42742
12	42								Raveen	937407
	BI18A	Bay of Islands Cycling Tour	10	9:45 AM	10:30 AM	3:30 PM	5:00	Royal Theatre, Deck 4 FWD.	4	10
13	10									
	BI05A	Coastal Sea Kayaking	30	9:45 AM	10:30 AM	2:30 PM	4:00	Royal Theatre, Deck 4 FWD.	4	30
14	30									
	BI09A	Historic Treaty House and Pioneer Settlement	90	10:15 AM	11:00 AM	3:00 PM	4:00	On the pier, ashore	20	90
15	46								Marino	926592
16	44								Fang	966188
	BI37B	Island and Cave Adventure	34	10:45 AM	11:30 AM	1:30 PM	2:00	On the pier, Allow 45 min to tender	15	34
17	34									
	BI05B	Coastal Sea Kayaking	18	11:15 AM	11:30 AM	3:30 PM	4:00	On the pier, Allow 45 min to tender	4	30
18	18									
	BI20A	Glow Worm Caves and Te Waimate	84	11:15 AM	11:30 AM	4:15 PM	4:45	On the Pier, ashore	20	80
19	42								Elena	402449
20	42								Candace	983604
	BI17A	Northland Maori Waka Encounter	59	11:15 AM	11:30 AM	2:30 PM	3:00	On the Pier, ashore	30	60
21	59									
	BI12B	Glow Worm Caves and Kauri Forest	82	11:30 AM	11:45 AM	3:45 PM	4:00	On the pier, Allow 45 min to tender	20	82
22	42									
23	40								Annie	932491
	BI16B	A Cruise to Zane Greys Hole in the Rock	96	11:45 AM	12:00 PM	3:00 PM	3:00	On The Pier, ashore	70	100
24	96								Lin	
	BI19A	Bay of Islands Lunch Cruise	45	12:15 PM	12:30 PM	4:00 PM	3:30	On the pier, ashore	10	45
25	45									
	BI18B	Bay of Islands Cycling Tour	10	12:15 PM	12:30 PM	5:30 PM	5:00	On The Pier, Ashora	4	10
26	10									
	BI12C	Glow Worm Caves and Kauri Forest	90	12:45 PM	1:00 PM	5:00 PM	4:00	On the pier, Allow 45 min to tender	20	86
27	46								Morten	282541
28	44									
	BI39B	Bay of Islands Nature Cruise and Paddleboarding	16	1:15 PM	1:30 PM	4:30 PM	3:00	On The Pier, Ashora	10	18
29	16									
Grand Total			1171							

RELEASED UNDER THE OFFICIAL INFORMATION ACT

**From:** Peter Heath  
**To:** [Mark Patterson](#)  
**Cc:** [Julia Paterson-Fourie](#); [Andrew Nock](#)  
**Subject:** PGF launch: Far North Holdings numbers  
**Date:** Wednesday, 21 February 2018 2:30:07 p.m.  
**Attachments:** [image004.png](#)  
[image005.png](#)

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Mark – good afternoon.

Andy was running through the funding numbers associated with the six projects submitted by Far North Holdings in this news release and he spotted that we had used an outdated figure for the airport project.

The numbers should all be as follows:

- Out of Scope [REDACTED]
- [REDACTED]
- [REDACTED]
- [REDACTED]
- [REDACTED]
- [REDACTED]
- [REDACTED]
- upgrades to Paihia and Russell wharves (**\$4.084 million** of PGF funding awarded), and
- Out of Scope [REDACTED]

Out of Scope [REDACTED]

Sorry to be a pain but for the sake of accuracy please would you confirm that these are the numbers you are using at your end? Correct airport number highlighted above.

Thanks and best regards - Peter

Peter Heath

**due north**  business communication & reputation management

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**From:** Mark Patterson [mailto:Mark.Patterson2@mbie.govt.nz]  
**Sent:** Wednesday, 21 February 2018 10:42 a.m.  
**To:** Stephanie Weller <stephanie.weller@mbie.govt.nz>  
**Cc:** Peter Heath [§ 9\(2\)\(a\)](#); Stephanie Weller <stephanie.weller@mbie.govt.nz>; Andy Nock <[§ 9\(2\)\(a\)](#)>  
**Subject:** Far North Holdings news release for review and input please [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi Steph,

Can you please cast your eye over the attached PR and approve please?

Julia and I have reviewed and made provided the necessary comments.

Thanks

Mark

**PLEASE NOTE I AM TRAVELLING THIS AFTERNOON AND WILL HAVE LIMITED EMAIL CONTACT**

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**From:** Peter Heath [mailto:[§ 9\(2\)\(a\)](mailto:§ 9(2)(a)@mbie.govt.nz)]  
**Sent:** Wednesday, 21 February 2018 10:27 a.m.  
**To:** Julia Paterson-Fourie  
**Cc:** Andrew Nock; Mark Patterson  
**Subject:** RE: Far North Holdings news release for review and input please [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi, Julia

Terrific stuff. Thank you.

Revised draft attached FYI. The projects remain highlighted to indicate that we should check against delivery – keen not to count chickens, etc!

I have rejigged the wording (marked up for easy reference) to take into account the non-PGF funding source on the one project. Let me know if you feel comfortable with this, will you?

What time is the announcement due, please? And will you let me have a copy of it at the same time as distribution to media so that I can check our facts against delivery before distributing our own release, hopefully within a few minutes of yours going out?

Thanks and best regards - Peter

Peter Heath

**due north**  business communication & reputation management

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**From:** Julia Paterson-Fourie [mailto:[Julia.Paterson-Fourie2@mbie.govt.nz](mailto:Julia.Paterson-Fourie2@mbie.govt.nz)]  
**Sent:** Wednesday, 21 February 2018 9:47 a.m.  
**To:** Peter Heath <[§ 9\(2\)\(a\)](mailto:§ 9(2)(a)@mbie.govt.nz)> Mark Patterson <[Mark.Patterson2@mbie.govt.nz](mailto:Mark.Patterson2@mbie.govt.nz)>  
**Cc:** Andrew Nock <[§ 9\(2\)\(a\)](mailto:§ 9(2)(a)@mbie.govt.nz)>  
**Subject:** RE: Far North Holdings news release for review and input please [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi Peter – thanks so much for sending this through and supporting the PGF launch with such a positive release!

Attached are our suggestions. Any questions, please let us know.

Cheers  
Julia

**Julia Paterson-Fourie**

PRINCIPAL ENGAGEMENT AND COMMUNICATIONS ADVISOR (LSE)  
Engagement, Communications and Ministerial Services branch  
Corporate, Governance and Information Group  
Ministry of Business, Innovation & Employment

[julia.paterson-fourie2@mbie.govt.nz](mailto:julia.paterson-fourie2@mbie.govt.nz) | Telephone: +64 (0)4 896 5482 | Mobile: +64 (0)21 835 108  
Level 4 15 Stout Street, PO Box 1473, Wellington 6140, New Zealand

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**From:** Peter Heath [mailto:[s.9\(2\)\(a\)](mailto:s.9(2)(a)@...)]  
**Sent:** Tuesday, 20 February 2018 6:31 p.m.  
**To:** Mark Patterson; Julia Paterson-Fourie  
**Cc:** Andrew Nock  
**Subject:** PGF: Far North Holdings news release for review and input please

Mark, Julia – good evening

Andy Nock at Far North Holdings has asked me to work with you on pre-approval of the attached news release in advance of the PGF announcement on Friday.

Highlighted sections include text that will have to be modified as soon as the details of the awards are known. Of course, we'd be delighted to work with you in firming this text up in advance (!)

Please would you let me know if there are any changes or additions you would like made?

Thanks and best regards  
Peter

Peter Heath



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[www.duenorthpr.co.nz](http://www.duenorthpr.co.nz) / [www.prblog.co.nz](http://www.prblog.co.nz)



-----Original Message-----

From: Andy Nock [mailto:[s.9\(2\)\(a\)](mailto:s.9(2)(a)@...)]  
Sent: Tuesday, 20 February 2018 5:31 p.m.  
To: Peter Heath <[s.9\(2\)\(a\)](mailto:s.9(2)(a)@...)>



Subject: FW: Comms introduction

Hi Peter,

Please review the attached and then send to Mark and Julia.

Thanks,

Andy

Andy Nock

Chief Executive  
Far North Holdings Limited  
PO Box 7  
Opua 0241  
s 9(2)(a)

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-----Original Message-----

From: Mark Patterson [<mailto:Mark.Patterson2@mbie.govt.nz>]

Sent: Tuesday, 20 February 2018 1:56 p.m.

To: Andy Nock <s 9(2)(a)>; Julia Paterson-Fourie <[Julia.Paterson-Fourie2@mbie.govt.nz](mailto:Julia.Paterson-Fourie2@mbie.govt.nz)>

Subject: Comms introduction

Hi Andy,

Will do the intro now. Julia can liaise with Peter on messaging before this Friday.

As noted in the phone, I ask you keep this to yourself.

Chat soon,

Mark

Mark Patterson  
Senior Advisor  
Regions and Cities

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**From:** Gwyn Thurlow  
**To:** David van der Zouwe; Mark Patterson  
**Cc:** Ward Tuite  
**Subject:** RE: Three Wharves [UNCLASSIFIED]  
**Date:** Friday, 22 June 2018 4:00:28 p.m.

Thanks David.

Points 1 and 2 are covered. I've added a new 'special term' to cover point 3 around financial covenants, as follows: *The Recipient must comply with all financial covenants in respect of its finance arrangements, and represents and warrants that entry into, and the performance of, its obligations under this Agreement, will not be or cause an "event of default" (however so described) under its financing arrangements.*

Mark, Ward,

Links are here:

- **Out of Scope**
- Russell: (<http://mako.wd.govt.nz/otcs/lisapi.dll?func=1&objaction=overview&objid=79630957>)

If you are happy, send them out to FNHL.

Kind regards  
Gwyn

**From:** David van der Zouwe  
**Sent:** Friday, 22 June 2018 12:02 p.m.  
**To:** Mark Patterson  
**Cc:** Gwyn Thurlow; Ward Tuite  
**Subject:** RE: Three Wharves [UNCLASSIFIED]

Hi there,

My comments:

- I note there is a comment in the Russell Wharf in relation to co-funding. My understanding is that all wharves rely on co-funding and will be pro-rated. So could you please remove the comment from Steph is the Russell Agreement;
- A condition is that we want to be across the appointment of contractors for all the wharves, so can we include that;
- Finally, far North have made the following representation in regard to their financial position:

Hi Mark,

**s 9(2)(b)(i)**

*The Company has effectively contributed, financially, as much as possible to show commitment to each development.*

Regards,

Andy

The Cabinet paper just asked that as part of the contracting process we confirm this position by taking a look at the calculation, so can we also include this as a pre-condition.

Perhaps we could say something like, *Confirmation that the company has contributed, financially, as much as possible to show commitment to each development.*

Nothing else from me, with these inclusions I am happy for this to go out

Cheers

David van

**From:** Mark Patterson  
**Sent:** Thursday, 21 June 2018 4:48 p.m.  
**To:** David van der Zouwe  
**Cc:** Gwyn Thurlow; Ward Tuite  
**Subject:** RE: Three Wharves [UNCLASSIFIED]

To start the elimination process...

David have you approved from you end both the Russell and **Out** wharf agreements?

Cheers

**From:** Gwyn Thurlow  
**Sent:** Thursday, 21 June 2018 4:41 p.m.  
**To:** Mark Patterson  
**Subject:** FW: Three Wharves [UNCLASSIFIED]

All 3 below – I think they went out? But I've got no visibility.

**From:** Gwyn Thurlow  
**Sent:** Friday, 8 June 2018 2:48 p.m.  
**To:** Stephanie Weller  
**Cc:** David van der Zouwe; Ward Tuite  
**Subject:** RE: Three Wharves [UNCLASSIFIED]

All,

**Out of Scope**

Kind regards  
Gwyn

**From:** Stephanie Weller  
**Sent:** Friday, 8 June 2018 2:39 p.m.  
**To:** Gwyn Thurlow  
**Cc:** David van der Zouwe; Ward Tuite  
**Subject:** RE: Three Wharves [UNCLASSIFIED]

Afternoon all

1. Paihia done and David reviewed – Ward - can go to client, you may have sent.

- Russell done by me – **David** please review - Far North Holdings Ltd re Russell Wharf Grant Funding Agreement.docx (<http://mako.wd.govt.nz/otcs/lisapi.dll?func=ll&objaction=overview&objid=79630957>)
- Out of Scope**

Both items 2 and 3 should be able to go together if we can.

Thanks  
Steph

---

**From:** Gwyn Thurlow  
**Sent:** Thursday, 7 June 2018 4:36 p.m.  
**To:** Stephanie Weller  
**Subject:** FW: Three Wharves [UNCLASSIFIED]

You're not on this, unless Ward, Mark, David or Pippa, passed them on...

---

**From:** Gwyn Thurlow  
**Sent:** Tuesday, 5 June 2018 11:31 a.m.  
**To:** David van der Zouwe  
**Cc:** Pippa Brown; Ward Tuite; Mark Patterson  
**Subject:** RE: Rotorua Big Moves and the Three Wharves [UNCLASSIFIED]

Hi David, Ward and Pippa,

Further to below, attached is the draft Russell wharf document. That's now all 3 drafted

For your ease, I've re-attached the previous two drafts to this email.

Kind regards,  
Gwyn

---

**From:** Gwyn Thurlow  
**Sent:** Tuesday, 5 June 2018 10:52 a.m.  
**To:** David van der Zouwe  
**Cc:** Pippa Brown; Ward Tuite; Mark Patterson  
**Subject:** RE: Rotorua Big Moves and the Three Wharves [UNCLASSIFIED]

David,

**Out of Scope**

Gwyn

---

**From:** David van der Zouwe  
**Sent:** Wednesday, 30 May 2018 2:41 p.m.  
**To:** Pippa Brown; Gwyn Thurlow  
**Subject:** **Out of Scope** and the Three Wharves [UNCLASSIFIED]

The documentation for **Out of Scope** and the Three Wharves.

Three Wharves – Mako Folder <http://mako.wd.govt.nz/OTCS/lisapi.dll/properties/79399964>

Let me know if you have any further queries.

Kind Regards

**David van der Zouwe**  
SENIOR COMMERCIAL LEADER

Commercial Proc, New Zealand Government Procurement & Property, Market Services  
Ministry of Business, Innovation & Employment

[David.VanDerZouwe@mbe.govt.nz](mailto:David.VanDerZouwe@mbe.govt.nz) | Telephone: +64 (0)4 801 3362 | Mobile: +64 21 804 611  
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New Zealand Government

**From:** [Ward Tuite](#)  
**To:** [Mark Patterson](#)  
**Subject:** RE: Draft Russell and [Out of Office] Wharf Investment Agreements [IN-CONFIDENCE:RELEASE EXTERNAL]  
**Date:** Tuesday, 17 July 2018 11:42:06 a.m.

---

Agreements sent to Andy 7.30 this morning

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**From:** Mark Patterson  
**Sent:** Tuesday, 17 July 2018 11:41 a.m.  
**To:** 'Andy Nock'; Ward Tuite  
**Subject:** RE: Draft Russell and [Out of Office] Wharf Investment Agreements [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi Andy,

Apologies I have been travelling the last week. I hope you had a good break.

Ward, has this been responded too? Pippa and David van der Zouwe may be of use here.

Let me know either way.

Cheers

Mark

---

**From:** Andy Nock [mailto:[s 9\(2\)\(a\)](#)]  
**Sent:** Wednesday, 11 July 2018 1:08 p.m.  
**To:** Mark Patterson; Ward Tuite  
**Subject:** RE: Draft Russell and [Out of Office] Wharf Investment Agreements [IN-CONFIDENCE:RELEASE EXTERNAL]

Hi Mark,

I am back, and noticed this was not dealt with in my absence.

Please find comments as they apply to [Out of Office] and are applicable to Russell.

Can you come back to me on these points.

We are looking to appoint a contractor within the next 7-10 days so appreciate if you could expedite this.

Cheers,

Andy

**Andy Nock**

**Chief Executive**  
**Far North Holdings Limited**  
**PO Box 7**  
**Opua 0241**  
**Ph:** [s 9\(2\)\(a\)](#)

Email: s 9(2)(a)

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**From:** Mark Patterson <[Mark.Patterson2@mbie.govt.nz](mailto:Mark.Patterson2@mbie.govt.nz)>

**Sent:** Friday, 22 June 2018 4:44 p.m.

**To:** Andy Nock <s 9(2)(a)>

**Cc:** Irwin Wilson <[Irwin@fnhl.co.nz](mailto:Irwin@fnhl.co.nz)>; Ward Tuite <[Ward.Tuite@mbie.govt.nz](mailto:Ward.Tuite@mbie.govt.nz)>

**Subject:** Draft Russell and Out of Scope Wharf Investment Agreements [IN-CONFIDENCE-RELEASE EXTERNAL]

Hi Andy,

I hope you are well.

I am acting for Ward in this instance, however please find attached draft investment agreements for:

Out of Scope

2. Russell Wharf

Cheers

Mark

**Mark Patterson**  
SENIOR ADVISOR  
Provincial Development Unit  
Ministry of Business, Innovation & Employment | Te Manatu Pakihi, Auahatanga me Nga Kaupapa a-Mahi

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**From:** Sue Dobbie | NZMT  
**To:** [Mark Patterson](#)  
**Cc:** [kylie@fnhl.co.nz](mailto:kylie@fnhl.co.nz); [Alison Page](#)  
**Subject:** Re: Meeting with Andy re Manea and other Far North Project Funding Agreements [UNCLASSIFIED]  
**Date:** Wednesday, 28 March 2018 8:52:42 a.m.  
**Importance:** High

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Kia ora Mark & Alison,

We haven't set a meeting time for this yet and wanted to propose 11-12:30pm on Tuesday 3 April as an option.

I had another thought and that is that the Working Group is meeting in Opua (which is on the opposite coast to the Hokianga) on Wednesday 4 April - scheduled from 11-4pm but unlikely to go that long. If you wanted to come up and join us for part of that meeting you would be welcome too.

At the risk of committing Andy and his team - this could be an opportunity to check out the locations for the proposed investment at Out of Scope and Russell (if you take a ferry). What do you think Kylie?

Nga mihi,  
Sue

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NZMT Logo



**Sue Dobbie**  
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**From:** Sue Dobbie | NZMT  
**Sent:** Friday, March 23, 2018 10:45:14 AM  
**To:** Mark Patterson  
**Cc:** [kylie@fnhl.co.nz](mailto:kylie@fnhl.co.nz); Alison Page  
**Subject:** Re: Meeting with Andy re Manea and other Far North Project Funding Agreements [UNCLASSIFIED]

Morena Mark,

Completely understand the caveats this is more of a discussion to make sure that we have together all the information we need and to talk about the shape of the funding agreements so that we can smooth the process of achieving the funding agreements rather than signing anything up. We would be happy to have Alison along too so we are all on the same page.



Have booked out the 3 April. Do you have a preference for time?

Nga mihi,  
Sue



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**From:** Mark Patterson <Mark.Patterson2@mbie.govt.nz>  
**Sent:** Wednesday, March 21, 2018 5:34:38 PM  
**To:** Sue Dobbie | NZMT  
**Cc:** kylie@fnhl.co.nz; Alison Page  
**Subject:** Re: Meeting with Andy re Manea and other Far North Project Funding Agreements [UNCLASSIFIED]

Hi Sue,

To caveat, it will be unlikely we will be in a position to sign agreements at this meeting.

Obviously also, agreements are still conditional on approving completed business cases which aren't due to be completed until later next week.

Happy to run through mocked up agreements in anticipation.

Alison from our investment team is helping with all the far north holding limited agreements.

The 3 April date may be better.

Mark Patterson  
Senior Advisor  
Regions and Cities

On 21/03/2018, at 5:07 PM, Sue Dobbie | NZMT <[sue@maoritourism.co.nz](mailto:sue@maoritourism.co.nz)> wrote:

Kia ora Mark,

Thanks for your time this morning - it is good to keep up the communication with things changing so rapidly.

I have checked with Kylie (Andy's EA) about his availability and wanted to check whether you would be available for approx 1.5 hrs to meet with us both

re funding agreements and milestones for projects in the Far North on **either Monday 26th March or Tuesday 3 April** (just after Easter)?

Nga mihi,  
Sue

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NZMT Logo



**Sue Dobbie**

**Contractor**

[NZ Maori Tourism](#)

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